

# CITY OF AVON PARK FBO/TERMINAL APRON REHABILITATION

CONFORMED DOCUMENTS  
DECEMBER 12, 2022  
FAA A.I.P. NO. 3-12-004-022-2022



**PROJECT LOCATION**  
N.T.S.

**ENGINEER OF RECORD**

JARED MORENG, FL P.E. NO. 81611 - FBO/TERMINAL APRON REHABILITATION

PREPARED FOR:



PREPARED BY:

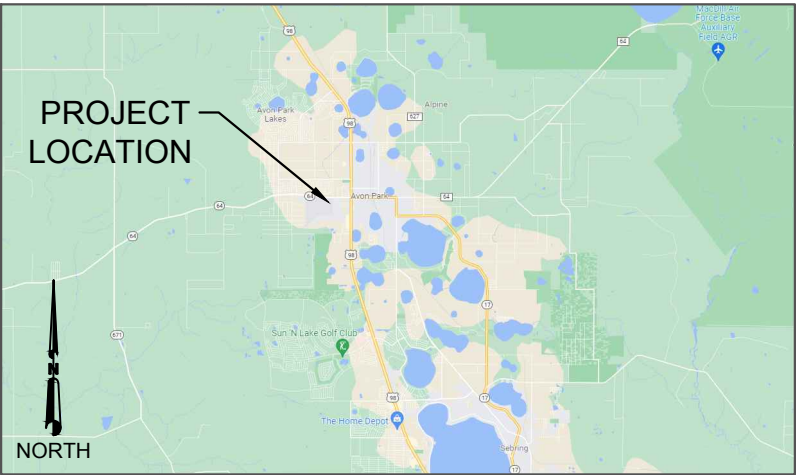
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KIMLEY-HORN AND ASSOCIATES, INC.  
REGISTRY NO. 35106  
655 NORTH FRANKLIN STREET, SUITE 150  
TAMPA, FL 36602-4447  
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**RECORD  
DRAWINGS**

RECORD DRAWINGS PREPARED BY KIMLEY-HORN AND ASSOCIATES, INC.  
HAVE BEEN BASED ON CONFORMED DOCUMENTS THROUGH ESI 01.  
AS-BUILT SURVEY PROVIDED BY AJAX PAVING INDUSTRIES OF FLORIDA.

ISSUED 10/12/2023



**LOCATION MAP**  
N.T.S.

**CITY OF AVON PARK**

CITY MANAGER  
MARK SCHRADER

CITY FINANCE DIRECTOR  
MELODY SAUERHAFFER

**UTILITIES**

DUKE ENERGY  
RHYS PIATT  
RHYS.PIATT@DUKE-ENERGY.COM  
321-278-0048

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DRAWN:	BJG
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INDEX OF DRAWINGS	
G001	

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BASE BID			
SUMMARY OF QUANTITIES			
ITEM NUMBER	DESCRIPTION	UNIT	QUANTITY
C-105-1	MOBILIZATION (10%)	LS	1
C-102-1	TEMPORARY AIR AND WATER POLLUTION, SOIL EROSION, AND SILTATION CONTROL	LS	1
M-102-1	MAINTENANCE OF TRAFFIC AND TEMPORARY CONSTRUCTION ITEMS	LS	1
M-103-1	PROJECT SURVEY AND STAKEOUT	LS	1
M-104-1	SAFETY AND SECURITY	LS	1
P-101-1	FULL DEPTH ASPHALT PAVEMENT REMOVAL	SY	8,200
P-101-2	FULL DEPTH CONCRETE PAVEMENT REMOVAL	SY	1,100
P-101-4	RPR DIRECTED CRACK REPAIR	LF	500
P-151-1	CLEARING AND GRUBBING	SY	3,600
P-152-1	UNCLASSIFIED EXCAVATION	CY	450
P-152-2	OFFSITE BORROW	CY	450
P-211-1	LIME ROCK BASE COURSE, 9" THICK	SY	9,300
P-211-2	SEPARATION GEOTEXTILE	SY	9,300
P-401-1	HOT MIXED ASPHALT PAVEMENT, SURFACE COURSE	TON	2,250
P-602-1	EMULSIFIED ASPHALT PRIME COAT	GAL	2,800
P-603-1	EMULSIFIED ASPHALT TACK COAT	GAL	750
P-610-1	CONCRETE DUMPSTER PAD	LS	1
P-620-1	PERMANENT AIRFIELD MARKINGS WITH TYPE III GLASS BEADS (YELLOW)	SF	225
T-904-1	SODDING & TOPSOILING	SY	3,600
L-100-1	UNDERGROUND UTILITY LOCATES	LS	1
L-100-2	GENERAL ELECTRICAL DEMOLITION	LS	1
L-100-3	RE-ESTABLISH ELECTRICAL SERVICE TO EXISTING T-HANGAR	LS	1

BASE BID NOTES:

1. THE BASE BID SCOPE OF WORK AND QUANTITIES SHOWN ON THIS SHEET ARE INCLUSIVE OF ALL WORK FOR PHASE 0 – ELECTRICAL WORK, PHASE 1A, PHASE 1B, AND PHASE 1C ONLY. THE LIMITS OF WORK FOR ALL PHASES INCLUDED IN THE BASE BID ARE GENERALLY SHOWN ON SHEET P112. ALL WORK ASSOCIATED WITH THESE PHASES CAN BE FOUND THROUGHOUT THE PLAN SET.

BID ADD ALTERNATE 1			
SUMMARY OF QUANTITIES			
ITEM NUMBER	DESCRIPTION	UNIT	QUANTITY
C-105-1	MOBILIZATION (10%)	LS	1
C-102-1	TEMPORARY AIR AND WATER POLLUTION, SOIL EROSION, AND SILTATION CONTROL	LS	1
M-102-1	MAINTENANCE OF TRAFFIC AND TEMPORARY CONSTRUCTION ITEMS	LS	1
M-103-1	PROJECT SURVEY AND STAKEOUT	LS	1
M-104-1	SAFETY AND SECURITY	LS	1
P-101-1	FULL DEPTH ASPHALT PAVEMENT REMOVAL	SY	1,700
P-101-2	FULL DEPTH CONCRETE PAVEMENT REMOVAL	SY	1,250
P-101-3	SALVAGE AND REINSTALL CONCRETE BOLLARD	EA	4
P-101-4	RPR DIRECTED CRACK REPAIR	LF	500
P-151-1	CLEARING AND GRUBBING	SY	600
P-152-1	UNCLASSIFIED EXCAVATION	CY	55
P-152-2	OFFSITE BORROW	CY	150
P-211-1	LIME ROCK BASE COURSE, 9" THICK	SY	2,900
P-211-2	SEPARATION GEOTEXTILE	SY	2,900
P-401-1	HOT MIXED ASPHALT PAVEMENT, SURFACE COURSE	TON	700
P-602-1	EMULSIFIED ASPHALT PRIME COAT	GAL	900
P-603-1	EMULSIFIED ASPHALT TACK COAT	GAL	250
P-608-1	EMULSIFIED ASPHALT SURFACE REJUVENATOR	SY	4,850
P-620-1	PERMANENT AIRFIELD MARKINGS WITH TYPE III GLASS BEADS (YELLOW)	SF	275
P-620-2	PERMANENT AIRFIELD MARKINGS WITH NO GLASS BEADS (BLACK)	SF	550
P-620-3	AIRIFELD MARKINGS REMOVAL	SF	825
T-904-1	SODDING & TOPSOILING	SY	500

BID ADD ALTERNATE 1 NOTES:

1. THE BID ADD ALTERNATE 1 SCOPE OF WORK AND QUANTITIES SHOWN ON THIS SHEET ARE INCLUSIVE OF ALL WORK FOR PHASE 2 AND PHASE 3 ONLY. THE LIMITS OF WORK FOR ALL PHASES INCLUDED IN BID ADD ALTERNATE 1 ARE GENERALLY SHOWN ON SHEET P113. ALL WORK ASSOCIATED WITH THESE PHASES CAN BE FOUND THROUGHOUT THE PLAN SET.

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SUMMARY OF QUANTITIES


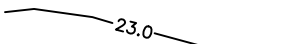
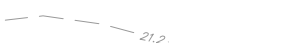
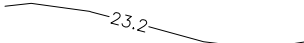

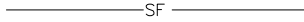
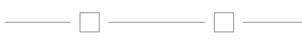






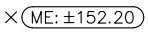

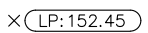

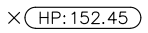

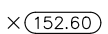











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ABBREVIATIONS

A	AMPERES	MES	MITERED END SECTION
AC	ACRE	MH	MANHOLE
ALS	APPROACH LIGHTING SYSTEM	MIN	MINIMUM
ALT	ALTERNATE	MISC	MISCELLANEOUS
AMSL	ABOVE MEAN SEA LEVEL	MON	MONUMENT
AOA	AIR OPERATIONS AREA	MULT	MULTIPLE
AR	AIRCRAFT RATED INLET	MSL	MEAN SEA LEVEL
ATCT	AIR TRAFFIC CONTROL TOWER	N	NORTH, NORTHING
ATPB	ASPHALT TREATED PERMEABLE BASE	NIC	NOT IN CONTRACT
AVO	AVON PARK EXECUTIVE AIRPORT	NO	NUMBER
AVI	AUTOMATED VEHICLE IDENTIFICATION	NPDES	NATIONAL POLLUTANT DISCHARGE
BC	BEGINNING OF CURVE		ELIMINATION SYSTEM
BIT	BITUMINOUS	NTS	NOT TO SCALE
BM	BENCH MARK	OC	ON CENTER
BMP	BEST MANAGEMENT PRACTICE	OD	OUTSIDE DIAMETER
BVC	BEGINNING OF VERTICAL CURVE	OFF	OFFSET
C	CONDUIT	PAPI	PRECISION APPROACH PATH INDICATOR
CAB	CRUSHED AGGREGATE BASE	PB	PULL BOX
CB	CATCH BASIN	PC	POINT OF CURVATURE
CBR	CALIFORNIA BEARING RATIO	PCC	PORTLAND CEMENT CONCRETE
CCAA	CHARLOTTE COUNTY AIRPORT	PI	POINT OF INTERSECTION
	AUTHORITY	PL	PROPERTY LINE
CDF	CONTROLLED DENSITY FILL	POC	POINT OF CURVE
CF	CUBIC FEET	POT	POINT OF TANGENT
CFS	CUBIC FEET PER SECOND	PRC	POINT OF REVERSE CURVE
CIP	CAST IRON PIPE	PRVC	POINT OF REVERSE VERTICAL CURVE
CL	CENTERLINE	PT	POINT OF TANGENCY
CO	CLEANOUT	PVC	POLYVINYL CHLORIDE
CONC	CONCRETE	PVI	POINT OF VERTICAL INTERSECTION
CONST	CONSTRUCT, CONSTRUCTION	PVMT	PAVEMENT
CONT	GROUND CONTOUR	PVT	POINT OF VERTICAL TANGENCY
COORD	COORDINATE	PSI	POUNDS PER SQUARE INCH
CSP	CORRUGATED STEEL PIPE	Q	RATE OF FLOW IN CFS
CSSO	CONTRACTOR'S SECURITY SAFETY	QCPA	QUALITY CONTROL PROGRAM
	OFFICER		ADMINISTRATOR
CQCP	CONTRACTOR QUALITY CONTROL	R	RADIUS
	PROGRAM	RC	REINFORCED CONCRETE
CQCPA	CONTRACTOR QUALITY CONTROL	RCP	REINFORCED CONCRETE PIPE
	PROGRAM ADMINISTRATOR	REF	REFERENCE
CY	CUBIC YARD	REIL	RUNWAY END IDENTIFIER LIGHTS
DIA	DIAMETER	REINF	REINFORCED, REINFORCEMENT
DWG	DRAWING	RGRCP	RUBBER GASKET REINFORCED
E	EAST, EASTING		CONCRETE PIPE
EA	EACH	RP	RADIUS POINT
EC	END OF CURVE	RPMP	REINFORCED PLASTIC MORTAR PIPE
EG	EXISTING GRADE	RPZ	RUNWAY PROTECTION ZONE
ELEV	ELEVATION	RSA	RUNWAY SAFETY AREA
ENG	ENGINEER, ENGINEERING	RT	RIGHT
EOP	EDGE OF PAVEMENT	RPR	RESIDENT PROJECT REPRESENTATIVE
ERCP	ELLIPTICAL REINFORCED CONCRETE	RVR	RUNWAY VISUAL RANGE
	PIPE	RW	RUNWAY
EV	ELECTRICAL VAULT	S	SLOPE
EVC	END OF VERTICAL CURVE	SB	SUBBASE
EQ	EQUAL	SC	SPIRAL TO CURVE
EX	EXIST	SD	STORM DRAIN
EXC	EXCAVATION	SEC	SECTION
EXP JT	EXPANSION JOINT	SF	SQUARE FEET
FAA	FEDERAL AVIATION ADMINISTRATION	SHLD	SHOULDER
FAR	FEDERAL AVIATION REGULATION	SPA	SPACES
FDEP	FLORIDA DEPARTMENT OF	SPEC	SPECIFICATIONS
	ENVIRONMENTAL PROTECTION	SS	SANITARY SEWER
FG	FINISHED GRADE	STA	STATION
FL	FLOW LINE	STD	STANDARD
FOD	FOREIGN OBJECT DEBRIS	STPD	SOIL TRACKING PREVENTION DEVICE
FS	FINISHED SURFACE	SWPPP	STORM WATER POLLUTION PREVENTION
FTCD	FODS TRACKOUT CONTROL DEVICE		PLAN
GA	GENERAL AVIATION	SY	SQUARE YARD
GR	GRADE	T, TAN	TANGENT TO CURVE
GSE	GROUND SERVICE EQUIPMENT	TG	TOP OF GRATE ELEVATION
HCL	HYDRAULIC GRADE LINE	TN	TRUE NORTH
HMA	HOT MIX ASPHALT	TOPO	TOPOGRAPHY
HOR	HORIZONTAL	TOW	TOP OF WALL
HYD	HYDRAULIC	TSA	TAXIWAY SAFETY AREA/TRANSPORTATION
INV	INVERT ELEVATION		SECURITY ADMINISTRATION
JB	JUNCTION BOX	TW	TAXIWAY
JT	JOINT	TYP	TYPICAL
L	LENGTH	VAR	VARIES, VARIABLE
LF	LINEAR FEET	VASI	VISUAL APPROACH SLOPE INDICATOR
LS	LUMP SUM	VC	VERTICAL CURVE
LT	LEFT	VER	VERIFY
MALS	MEDIUM INTENSITY APPROACH LIGHT	VERT	VERTICAL
	SYSTEM	VIF	VERIFY IN FIELD
MALSR	MEDIUM INTENSITY APPROACH LIGHT	VOL	VOLUME
	SYSTEM W/RAILS	VOR	VHF OMNIDIRECTIONAL RANGE
MAX	MAXIMUM	W/	WITH
ME	MATCH EXISTING	W	WATERLINE

LEGEND

EXISTING		PROPOSED	
	CONTOUR—MAJOR		CONTOUR—MAJOR
	CONTOUR—MINOR		CONTOUR—MINOR
	STORM SEWER		SILT FENCE
	AOA FENCE		AIRCRAFT MOVEMENT
	BOLLARD		TYPE 1 LOW-PROFILE BARRICADE (FOR THE STATED WORK AREA)
	JUNCTION CAN		INLET PROTECTION
	TAXIWAY EDGE LIGHT		MATCH EXISTING SPOT ELEVATION
	AIRFIELD SIGN		LOW POINT ELEVATION
	TIE-DOWN ANCHOR		HIGH POINT ELEVATION
	LIGHT POLE		PROPOSED SPOT ELEVATION
	GUY WIRE		PAVEMENT SPOT ELEVATION
	GRATE INLET — STORM WATER		MATCH EXISTING ELEVATION
	STORM OUTLET		CONSTRUCTION TRAFFIC SIGN
	EXISTING SPOT ELEVATION		SECURITY GUARD
	UTILITY POLE		HAUL ROUTE
			FODS TRACKOUT CONTROL DEVICE OR APPROVED EQUAL (FTCD)

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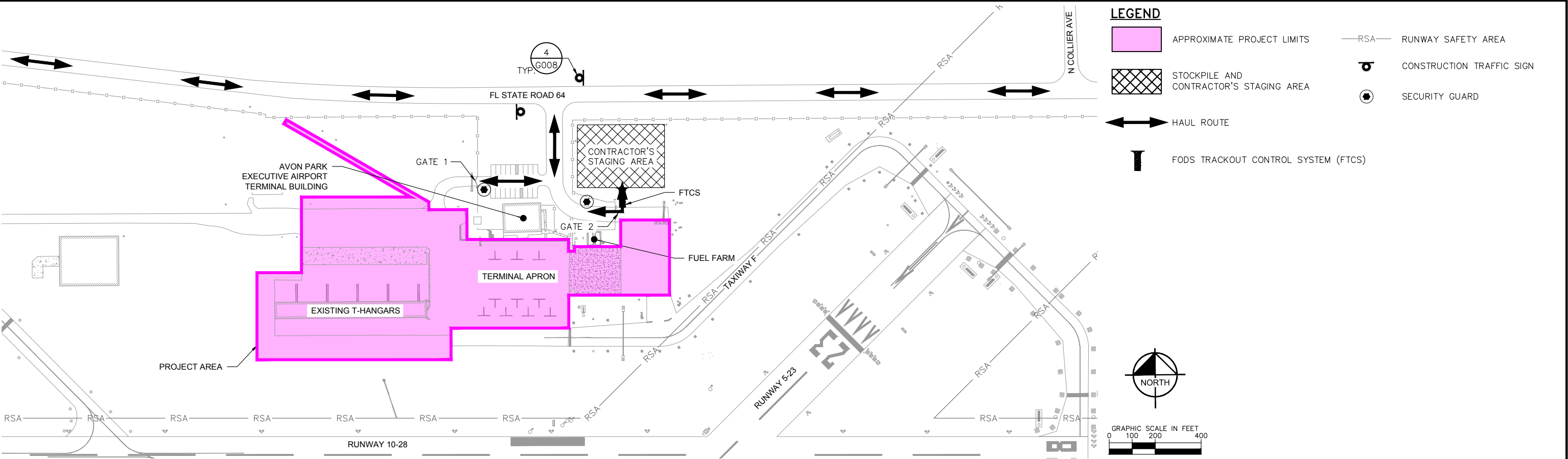
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LEGEND AND ABBREVIATIONS

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CONTRACTOR STAGING AND HAUL ROUTE NOTES:

1. CONTRACTOR'S ACCESS TO THE CONSTRUCTION SITE AND HAUL ROADS ARE IDENTIFIED ON THE PLANS. CONTRACTOR SHALL NOT USE ANY ACCESS OR HAUL ROADS OTHER THAN THOSE SHOWN UNLESS APPROVED BY RPR. CONTRACTOR MUST SUBMIT SPECIFIC PROPOSED ROUTES ASSOCIATED WITH CONSTRUCTION ACTIVITIES TO THE OWNER FOR EVALUATION AND APPROVAL AS PART OF THE SAFETY PLAN BEFORE BEGINNING CONSTRUCTION ACTIVITIES. THESE PROPOSED ROUTES MUST ALSO PROVIDE PROVISIONS TO PREVENT INADVERTENT ENTRY TO MOVEMENT AREAS. CONTRACTOR SHALL PAY SPECIAL ATTENTION TO ENSURE THAT EMERGENCY VEHICLE RIGHT OF WAY ON ACCESS AND HAUL ROADS IS NOT IMPEDED AT ANY TIME AND THAT CONSTRUCTION TRAFFIC ON HAUL ROADS DOES NOT INTERFERE WITH NAVAIDS OR APPROACH SURFACES OF OPERATIONAL RUNWAYS.
2. MAINTENANCE OF TRAFFIC IS THE CONTRACTOR'S RESPONSIBILITY. ALL PUBLIC AND AIRPORT ROADS ARE TO REMAIN OPEN TO TWO-WAY TRAFFIC AT ALL TIMES UNLESS APPROVED IN WRITING BY THE OWNER OR RPR.
3. IT IS THE CONTRACTOR'S RESPONSIBILITY TO COORDINATE ALL OFF-AIRPORT HAUL ROUTES (HIGHWAYS, COUNTY AND LOCAL ROADS, OR CITY STREETS) WITH THE APPLICABLE AGENCY HAVING JURISDICTION OVER THE AFFECTED ROUTE.
4. ALL ON-AIRPORT HAUL ROUTES SHALL BE JOINTLY EXAMINED AND VIDEO DOCUMENTED BY THE CONTRACTOR AND RPR BEFORE THE START OF CONSTRUCTION.
5. DAMAGE TO ANY PAVEMENTS OR PROPERTY CAUSED BY THE CONTRACTORS'S HAULING OR CONSTRUCTION EQUIPMENT SHALL BE REPAIRED TO THE FULL SATISFACTION OF THE OWNER AT THE CONTRACTOR'S SOLE EXPENSE.
6. HAULING WILL BE REQUIRED THROUGH EXISTING AIRCRAFT APRONS THROUGHOUT THE DURATION OF THE PROJECT. AT THE CONCLUSION OF EACH WORK SHIFT THE HAUL ROUTE SHALL BE SWEEPED WITH A VACUUM SWEEPER AND CHECKED FOR FOREIGN OBJECT DEBRIS (FOD) PRIOR TO OPENING AIRFIELD FACILITIES TO AIRCRAFT TRAFFIC. THE CONTRACTOR SHALL MAKE PROVISIONS TO HAVE A POWERED BROOM AND VACUUM SWEEPER ON SITE AT ALL TIMES. THE RPR SHALL INSPECT AREAS FOR FOD WITH THE CONTRACTOR PRIOR TO OPENING THE AFFECTED AIRPORT PAVEMENT.
7. THE CONTRACTOR'S STAGING AREA WILL BE LOCATED AS SHOWN ON THIS SHEET. THE CONTRACTOR SHALL UTILIZE THESE LOCATIONS THROUGHOUT CONSTRUCTION. THE CONTRACTOR SHALL SECURE THE PERIMETER OF THE STAGING AREA WITH A TEMPORARY SECURITY FENCE AND CONTROLLED ACCESS GATE. THE CONTRACTOR IS RESPONSIBLE FOR ESTABLISHING THEIR OWN UTILITIES, TEMPORARY FENCING, LIGHTING, AND OTHER ITEMS NECESSARY FOR THE STAGING AREA. COST FOR TEMPORARY UTILITIES AND LIGHTING SHALL BE INCLUDED IN THE BID UNIT PRICE FOR MOBILIZATION, C-105.
8. THE CONTRACTOR'S VEHICLES AND EQUIPMENT SHALL BE RESTRICTED TO THE CONSTRUCTION LIMITS AND THE CONTRACTOR'S STAGING AREA ONLY. DESIGNATED PARKING FOR THE CONTRACTOR'S AND SUBCONTRACTOR'S EMPLOYEE VEHICLES SHALL BE RESTRICTED TO THE CONTRACTOR'S STAGING AREA OR

- OTHER LOCATIONS APPROVED BY THE RPR. THE CONTRACTOR SHALL BE RESPONSIBLE FOR TRANSPORTING ALL PERSONNEL BETWEEN THE STAGING AREA(S) AND THE PROJECT WORK AREA(S). OVERNIGHT EQUIPMENT STORAGE SHALL BE AT THE CONTRACTOR'S STAGING AREA(S) ONLY UNLESS OTHERWISE APPROVED IN WRITING BY THE RPR.
9. THE CONTRACTOR SHALL SET UP TRAFFIC CONES TO DELINEATE HAUL ROUTES ACROSS CLOSED AIRFIELD PAVEMENTS DURING CONSTRUCTION HOURS. CONTRACTOR SHALL REMOVE THE TRAFFIC CONES AT THE CONCLUSION OF EACH WORK DAY/NIGHT TO THE STAGING AREAS.
10. ALL CONSTRUCTION EQUIPMENT SHALL HAVE APPROPRIATE FLAGS, MARKINGS AND LIGHTING PER THE LATEST VERSION FAA AC 150/5210-5 "PAINTING MARKING AND LIGHTING OF VEHICLES USED ON AN AIRPORT".
11. CONTRACTOR SHALL GIVE THE RIGHT OF WAY TO THE PUBLIC WHILE USING PUBLIC ROADS. CONTRACTOR SHALL NOT BLOCK ROADWAYS AND ACCESS TO BUSINESSES. CONTRACTOR SHALL PROVIDE FLAGMEN TO DIRECT TRAFFIC IF TRUCKS OR VEHICLES ARE TEMPORARILY PARKED ON PUBLIC OR AIRPORT-OWNED ROADWAYS. MAINTENANCE OF TRAFFIC SHALL MEET ALL LOCAL REQUIREMENTS AND GUIDELINES. CONTRACTOR SHALL CONTACT THE CITY OF AVON PARK, HIGHLANDS COUNTY, AND FLORIDA DEPARTMENT OF TRANSPORTATION FOR ANY PERMIT REQUIRED FOR THE USE OF PUBLIC ROADS.
12. ANY USE OF PRIVATE PROPERTY BY THE CONTRACTOR, INCLUDING PRIVATE ROADWAYS, SHALL BE COORDINATED IN ADVANCE WITH THE PROPERTY OWNER FOR APPROVAL.
13. CONTRACTOR SHALL BE RESPONSIBLE FOR ANY FINES FOR IMPROPER OR ILLEGAL USE OF PUBLIC OR PRIVATE PROPERTY IN THE PERFORMANCE OF THIS WORK AT THE CONTRACTOR'S SOLE EXPENSE.
14. CONTRACTOR'S STAGING AND STOCKPILING AREAS SHALL BE CLEARLY MARKED AND LIGHTED FOR THE DURATION OF CONSTRUCTION. TEMPORARY CONSTRUCTION AREA AND FLOOD LIGHTING SHALL BE PLACED IN A MANNER TO POINT AWAY FROM THE RUNWAY ENDS.
15. THE CONTRACTOR IS RESPONSIBLE FOR SECURING THEIR STOCKPILE AND STAGING AREAS, EQUIPMENT AND MATERIALS LOCATED ON AIRPORT PROPERTY.
16. THE CONTRACTOR'S STAGING AREA AND TEMPORARY HAUL ROADS SHALL BE REMOVED UPON COMPLETION OF CONSTRUCTION AND THE DISTURBED AREA SHALL BE RESTORED TO THE ORIGINAL CONDITION OR BETTER. TEMPORARY HAUL ROADS AND ASSOCIATED IMPROVEMENTS SHALL BE CONSIDERED INCIDENTAL TO PAY ITEM C-105 MOBILIZATION.
17. CONTRACTOR SHALL ENSURE THAT VEHICLES/EQUIPMENT MOVING TO AND FROM THE SITE AND STAGING AREAS. DO NOT TRACK PRIME/TACK COAT, DIRT, MUD, OR DEBRIS ON PUBLIC OR AIRPORT PAVEMENTS. ANY TRACKING OF SUCH MATERIAL SHALL BE REMOVED IMMEDIATELY BY THE CONTRACTOR AT NO COST TO THE OWNER.

18. CONTRACTOR SHALL STOP AT ALL ACTIVE AIRCRAFT MOVEMENT AREAS AND SHALL NOT PROCEED INTO AN ACTIVE AIRCRAFT MOVEMENT AREA WITHOUT ADVANCED COORDINATION WITH THE AIRPORT MANAGEMENT/OPERATIONS STAFF/RPR.
19. MOTORIZED VEHICLES AND EQUIPMENT OPERATING IN THE AOA SHALL NOT EXCEED THE POSTED SPEED LIMIT OR 15 MPH, WHICHEVER IS LESS.
20. AIRCRAFT SHALL HAVE RIGHT-OF-WAY OVER ALL MOTORIZED VEHICLES AND EQUIPMENT AT ALL TIMES.
21. VEHICLE DRIVERS MUST CONFIRM BY PERSONAL OBSERVATION THAT NO AIRCRAFT IS APPROACHING THEIR POSITION.
22. CONTRACTORS MUST TAKE CARE TO MAINTAIN A HIGH LEVEL OF SAFETY AND SECURITY DURING CONSTRUCTION WHEN ACCESS POINTS ARE CREATED IN THE SECURITY FENCING TO PERMIT THE PASSAGE OF CONSTRUCTION VEHICLES OR PERSONNEL. TEMPORARY GATES SHALL BE EQUIPPED SO THEY CAN BE SECURELY CLOSED AND LOCKED TO PREVENT ACCESS BY ANIMALS AND PEOPLE. PROCEDURES SHALL BE IN PLACE TO ENSURE THAT ONLY AUTHORIZED PERSONS AND VEHICLES HAVE ACCESS TO THE AOA AND TO PROHIBIT "PIGGYBACKING" BEHIND ANOTHER PERSON OR VEHICLE. CONTRACTOR MUST IMMEDIATELY CONTACT THE RPR AND AIRPORT MANAGEMENT/OPERATIONS UPON UNAUTHORIZED ENTRY.
23. CONTRACTOR AND SUBCONTRACTORS SHALL NOT ACCESS RESTRICTED AREAS ON THE AIRFIELD EXCEPT THE CONSTRUCTION LIMITS WITHIN THE STAGING AREAS, HAUL ROUTES, STORAGE AND STOCKPILES, AND THE CONSTRUCTION SITE AS APPROVED BY THE OWNER.
24. CONTRACTOR SHALL ACQUIRE, IF NECESSARY, APPLICABLE PERMITS FROM LOCAL AGENCIES FOR ANY MODIFICATION TO THE STAGING AREA SUCH AS SECURITY FENCING, GRADING AND DRAINAGE OR INSTALLING UTILITIES SUCH AS WATER, SEWER, AND POWER. CONTRACTOR'S STAGING AREA, ACCESS ROADS AND OTHER AREAS USED BY THE CONTRACTOR SHALL BE RETURNED TO THEIR ORIGINAL CONDITION AFTER THE CONSTRUCTION IS COMPLETE AND ALL EQUIPMENT, TRAILERS, ETC. ARE REMOVED OFF THE AIRPORT PROPERTY. THE COST FOR THIS WORK SHALL BE INCLUDED IN THE UNIT PRICE FOR MOBILIZATION, C-105.
25. CONSTRUCTION PERMITTING OF BATCH PLANT SHALL BE THE CONTRACTORS RESPONSIBILITY (IF APPLICABLE) AND CONTRACTOR TO COORDINATE WITH RPR ON LOCATION OF BATCH PLANT (IF APPLICABLE)

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AVON PARK, FL

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CONTRACT LAYOUT PLAN

G004

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GENERAL NOTES

1. THE RPR SHALL HAVE THE AUTHORITY TO SUSPEND THE WORK WHOLLY, OR IN PART, FOR SUCH PERIOD OR PERIODS AS THEY MAY DEEM NECESSARY, DUE TO UNSUITABLE WEATHER, OR SUCH OTHER CONDITIONS AS ARE CONSIDERED UNFAVORABLE FOR THE EXECUTION OF THE WORK, OR FOR SUCH TIME AS IS NECESSARY DUE TO THE FAILURE ON THE PART OF THE CONTRACTOR TO CARRY OUT ORDERS GIVEN OR PERFORM ANY OR ALL PROVISIONS OF THE CONTRACT.
2. THE OWNER AND ENGINEER WILL NOT BE RESPONSIBLE FOR THE CONTRACTOR’S MEANS, METHODS, TECHNIQUES, SEQUENCES, OR PROCEDURES OF CONSTRUCTION OR THE SAFETY PRECAUTIONS INCIDENT THERETO. CONTENT CONTAINED IN THESE PLANS REGARDING THESE MATTERS IS PROVIDED FOR INFORMATION ONLY.
3. THE QUANTITIES SHOWN ON THE BID DOCUMENTS ARE APPROXIMATE AND ARE FOR THE SOLE PURPOSE OF COMPARING BIDS. CONTRACTOR SHALL BE PAID FOR ACTUAL QUANTITIES MEASURED IN THE FIELD AS APPROVED BY THE RPR, UNLESS DEFINED AS A LUMP SUM BID ITEM.
4. CONTRACTOR SHALL SUBMIT A CONTRACTOR QUALITY CONTROL PROGRAM (CQCP) FOR ENGINEER’S REVIEW AND ACCEPTANCE 14 DAYS PRIOR TO THE START OF WORK. THE CQCP SHALL INCLUDE THE CONTRACTOR’S PROGRAM OF QUALITY CONTROL FOR ALL ITEMS OF WORK PER THE CONSTRUCTION SPECIFICATION. A CONTRACTOR QUALITY CONTROL PROGRAM ADMINISTRATOR (CQCPA) SHALL BE DESIGNATED BY THE CONTRACTOR AND APPROVED BY THE ENGINEER. ALL COSTS ASSOCIATED WITH PREPARING THE CQCP AND PERFORMANCE OF QUALITY CONTROL TESTING SHALL BE INCLUDED IN THE UNIT PRICE FOR THE APPLICABLE WORK ITEMS AND PAID FOR BY THE CONTRACTOR. THE OWNER WILL PAY FOR AND PERFORM QUALITY ASSURANCE/ACCEPTANCE TESTING IN CONJUNCTION WITH THE CONTRACTOR’S QUALITY CONTROL TESTING PER SPECIFICATIONS.
5. CONTRACTOR SHALL PREPARE A STORMWATER POLLUTION AND PREVENTION PLAN (SWPPP) AND FILE A NOTICE OF INTENT WITH THE FLORIDA DEPARTMENT OF ENVIRONMENTAL PROTECTION (FDEP) PRIOR TO THE START OF ANY CONSTRUCTION THAT WOULD AFFECT THE PROJECT SITE. THE CONTRACTOR SHALL FILE AND PAY FOR THE NPDES NOI UNDER C–105 MOBILIZATION.
6. THE RPR WILL NOT ESCORT THE CONTRACTOR DURING CONSTRUCTION.
7. THE CONTRACTOR SHALL CONDUCT THEIR WORK IN SUCH MANNER AS TO PREVENT THE ENTRY OF FUELS, OILS, BITUMINOUS MATERIALS, CHEMICALS, SEWAGE OR OTHER HARMFUL MATERIALS INTO STREAMS, RIVERS, LAKES OR RESERVOIRS.
8. CONTRACTOR SHALL MEET ALL APPLICABLE LOCAL AND FEDERAL LAWS WHEN DISPOSING ANY OILS OR OTHER FLUIDS REMOVED FROM EQUIPMENT BEING SERVICED ON AIRPORT PROPERTY. CONTRACTOR SHALL IMMEDIATELY CLEAN ANY SPILLS, CONTACT THE RPR, AND LEGALLY DISPOSE OF THE MATERIAL OFF AIRPORT PROPERTY.
9. MINIMAL POSSIBLE AREAS OF OPEN GRADING, BORROW OR AGGREGATE EXCAVATION SHALL BE ONLY EXPOSED AT ONE TIME, CONSISTENT WITH THE PROGRESS OF THE WORK.
10. CONSTRUCTION OPERATIONS SHALL BE CONDUCTED IN SUCH A MANNER AS TO REDUCE EROSION TO THE PRACTICABLE MINIMUM AND TO PREVENT DAMAGING SILTATION OF WATER COURSES, STREAMS, LAKES OR RESERVOIRS. THE SURFACE AREA OF ERODIBLE LAND, EITHER ON OR OFF THE AIRPORT SITE, EXPOSED TO THE ELEMENTS BY CLEARING, GRUBBING OR GRADING OPERATIONS, INCLUDING GRAVEL PITS, WASTE OR DISPOSAL AREAS AND HAUL ROADS, REQUIRED TO BE OPEN AT ANY TIME FOR THIS CONTRACT, SHALL BE SUBJECT TO APPROVAL OF THE RPR. THE DURATION OF SUCH EXPOSURE PRIOR TO FINAL TRIMMING AND FINISHING OF THE AREAS SHALL BE HELD TO THE MINIMUM EXTENT PRACTICAL. THE RPR SHALL HAVE FULL AUTHORITY TO ORDER THE SUSPENSION OF GRADING AND OTHER OPERATIONS PENDING ADEQUATE AND PROPER PERFORMANCE OF FINISHING AND MAINTENANCE WORK OR TO RESTRICT THE TRIMMING OF ERODIBLE LAND EXPOSED TO THE ELEMENTS. CONTRACTOR SHALL FILE AND PAY FOR NPDES NOI.
11. THE DISTURBANCE OF LANDS AND WATERS THAT ARE OUTSIDE THE LIMITS OF CONSTRUCTION AS SHOWN ON THE PLANS IS PROHIBITED, EXCEPT AS FOUND NECESSARY AND APPROVED BY THE RPR.
12. CONTRACTOR IS RESPONSIBLE FOR STAKING THE CONSTRUCTION SITE. CONTRACTOR SHALL IMMEDIATELY NOTIFY THE RPR OF ANY DISCREPANCY BETWEEN THE CONSTRUCTION DOCUMENTS AND FIELD CONDITIONS. COST OF CONSTRUCTION STAKING AND MEASUREMENTS SHALL BE INCLUDED IN THE UNIT PRICE FOR THE VARIOUS ITEMS OF WORK.
13. CONTRACTOR SHALL EXAMINE EXISTING ACCESS ROADS AND AIRFIELD PAVEMENTS TO DETERMINE IF THEY ARE CAPABLE OF SUSTAINING LOADS FROM VEHICLES AND CONSTRUCTION EQUIPMENT. ANY DAMAGE TO EXISTING PAVEMENTS SHALL BE REPAIRED BY THE CONTRACTOR AT NO EXPENSE TO THE OWNER. CONTRACTOR IS RESPONSIBLE TO ENSURE THAT ANY HAUL ROUTE IDENTIFIED IS OPERATIONAL FOR CONSTRUCTION TRAFFIC.
14. EXISTING PAVEMENT OR OTHER EXISTING WORK NOT SPECIFIED FOR REMOVAL WHICH IS TEMPORARILY REMOVED, DAMAGED OR IN ANY WAY DISTURBED OR ALTERED BY THE CONTRACTOR SHALL BE REPAIRED, PATCHED, OR REPLACED BY THE CONTRACTOR TO THE COMPLETE SATISFACTION OF THE OWNER/RPR/ENGINEER AT NO COST TO THE OWNER.
15. THERE ARE EXISTING UNDERGROUND UTILITIES IN THE PROJECT WORK AREA. THE LOCATION, ELEVATION AND DIMENSIONS OF THE EXISTING UTILITIES, STRUCTURES AND OTHER FEATURES ARE SHOWN IN ACCORDANCE WITH THE BEST INFORMATION AVAILABLE AT THE TIME OF THE PREPARATION OF THESE PLANS BUT DO NOT PURPORT TO BE ABSOLUTELY ACCURATE. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO HAVE EVERY UNDERGROUND UTILITY LOCATED, FLAGGED AND IDENTIFIED PRIOR TO CONSTRUCTION AND COORDINATED WITH RPR/AIRPORT MANAGEMENT/OPERATIONS. THE CONTRACTOR SHALL CAPTURE AND RECORD ANY CONDITIONS THAT DEVIATE FROM THE CONSTRUCTION PLANS ON THE AS–BUILT PLANS FOR SUBMITTAL TO THE RPR DURING THE PERFORMANCE OF THE PROJECT.
16. IF APPLICABLE TO THE PROJECT SCOPE OF WORK, THE LOCATION OF EXISTING FAA UNDERGROUND CABLES WILL BE FLAGGED ONE TIME BY THE AIRWAYS FACILITIES SECTOR OFFICE PERSONNEL, IF APPLICABLE, THROUGH COORDINATION WITH THE RPR. THESE FLAGS SHALL BE PROTECTED AND MAINTAINED BY THE CONTRACTOR AT ALL TIMES. IF THE FLAGS ARE LOST OR REMOVED BY THE CONTRACTOR THEY WILL BE FLAGGED AGAIN AT THE CONTRACTOR’S EXPENSE. THE CONTRACTOR IS RESPONSIBLE FOR OBTAINING UTILITY LOCATIONS OF ALL NON–FAA UNDERGROUND CABLES AND OTHER UTILITIES. ALL UNDERGROUND CABLES AND UTILITIES SHALL BE PROTECTED AND DAMAGES REPAIRED EXPEDITIOUSLY BY THE CONTRACTOR AT NO COST TO THE OWNER.

GENERAL NOTES (CONTINUED)

17. DAMAGE TO OTHER AIRPORT FACILITIES SUCH AS LIGHTING, SIGNAGE, NAVIGATIONAL AIDS, FAA CABLES, BUILDINGS, DRAINAGE STRUCTURES, FUELING STRUCTURES, FENCING, OTHER UTILITIES, ETC. SHALL BE REPORTED TO THE RPR AND THE FAA AND REPAIRED IMMEDIATELY BY THE CONTRACTOR AT NO COST TO THE OWNER.
18. CONTRACTOR SHALL COORDINATE AND COOPERATE WITH OTHER CONTRACTORS WHO MIGHT BE WORKING CONCURRENTLY AT THE AIRPORT. ANY CONFLICTS IN PERFORMING WORK SHALL BE IMMEDIATELY BROUGHT TO THE ATTENTION OF THE RPR FOR RESOLUTION.
19. ALL CONSTRUCTION WASTE SHALL BE DISPOSED OF IN A LEGAL MANNER OFF AIRPORT PROPERTY. CONTRACTOR SHALL ACQUIRE ANY REQUIRED PERMITS FOR DISPOSAL OF THIS MATERIAL.
20. IF CONTRACTOR ENCOUNTERS SOIL STAINS, UNUSUAL ODORS, OR BURIED WASTE, WORK SHALL BE STOPPED AND THE RPR SHALL BE NOTIFIED IMMEDIATELY FOR RESOLUTION PRIOR TO CONTINUING WORK IN THE AFFECTED AREA. CONTRACTOR SHALL BE RESPONSIBLE FOR TESTING, HANDLING, DISPOSING, AND ALL WORK NECESSARY TO MITIGATE CONTAMINATED SOILS.
21. IF CONSTRUCTION ACTIVITIES OCCUR AT NIGHTTIME THE CONTRACTOR SHALL PROVIDE SUFFICIENT LIGHTING CAPABLE OF FULLY LIGHTING THE WORK AREAS DURING CONSTRUCTION ACTIVITIES. THE CONTRACTOR SHALL COORDINATE THE DIRECTION AND ANGLE OF THE LIGHTS WITH THE OWNER AND RPR TO PREVENT IMPAIRING AIRPORT OPERATIONS.
22. WHEN NECESSARY, CERTAIN OPERATIONS SHALL BE DELAYED UNTIL PROPER WIND OR CLIMATIC CONDITIONS EXIST TO DISSIPATE OR INHIBIT POTENTIAL POLLUTANTS TO THE SATISFACTION OF THE RPR.
23. CONTRACTOR SHALL INSPECT AND CLEAN ANY RUNWAYS, TAXIWAYS, AND APRONS PRIOR TO THE AIRPORT RE–OPENING THESE AIRFIELD PAVEMENTS. REMOVAL OF BARRICADES SHALL BE THE LAST ORDER OF OPERATIONS BEFORE ANY AIRFIELD PAVEMENT IS OPENED UP TO TRAFFIC AND SHALL BE DONE SO ONLY UPON APPROVAL FROM THE OWNER AND RPR.
24. UPON COMPLETION OF THE WORK AND BEFORE ACCEPTANCE AND FINAL PAYMENT WILL BE MADE, THE CONTRACTOR SHALL RETURN, IF APPLICABLE, ALL VEHICLE PASSES, REMOVE FROM THE SITE ALL MACHINERY, EQUIPMENT, SURPLUS AND DISCARDED MATERIALS, RUBBISH, AND TEMPORARY STRUCTURES. MATERIAL CLEARED FROM THE SITE AND DEPOSITED ON ADJACENT PROPERTY WILL NOT BE CONSIDERED AS HAVING BEEN DISPOSED OF SATISFACTORILY, UNLESS THE CONTRACTOR HAS OBTAINED THE WRITTEN PERMISSION OF SUCH PROPERTY OWNER.

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STAGING AND HAUL ROUTE NOTES

G005



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PROJECT AND SAFETY NOTES

1. CONTRACTOR SHALL BE FAMILIAR WITH THE FOLLOWING FAA SAFETY PROVISIONS AND IMPLEMENT THESE REQUIREMENTS DURING CONSTRUCTION. THE LATEST COPIES OF THESE GUIDELINES CAN BE OBTAINED ON-LINE AT (HTTP://WWW.FAA.GOV/REGULATIONS\_POLICIES).
- a. FAA ADVISORY CIRCULAR AC 150/5210-5, "PAINTING, MARKING AND LIGHTING OF VEHICLES USED ON AN AIRPORT"

b. FAA ADVISORY CIRCULAR 150/5210-24, "AIRPORT FOREIGN OBJECT DEBRIS (FOD) MANAGEMENT"

c. FAA ADVISORY CIRCULAR 150/5370-2, "OPERATIONAL SAFETY ON AIRPORTS DURING CONSTRUCTION"

d. CODE OF FEDERAL REGULATIONS, 14 CFR PART 77, "SAFE EFFICIENT USE AND PRESERVATION OF THE NAVIGABLE AIRSPACE"
2. CONTRACTOR SHALL COMPLY WITH THE CONSTRUCTION SAFETY AND PHASING PLAN ASSOCIATED WITH THE CONSTRUCTION PROJECT AND ENSURE THAT CONSTRUCTION PERSONNEL ARE FAMILIAR WITH SAFETY PROCEDURES AND REGULATIONS OF THE AIRPORT.
3. CONTRACTOR SHALL PROVIDE A CONTRACTOR SECURITY SAFETY OFFICER (CSSO) FAMILIAR WITH AIRPORT SAFETY TO MONITOR CONSTRUCTION ACTIVITIES AND COORDINATE AN IMMEDIATE RESPONSE TO CORRECT ANY CONSTRUCTION RELATED ACTIVITY THAT MAY ADVERSELY AFFECT THE OPERATIONAL SAFETY TO THE AIRPORT. THE CSSO SHALL BE RESPONSIBLE FOR ALL SAFETY PRECAUTIONS PRIOR TO AND DURING THE DURATION OF THE CONSTRUCTION. THE CSSO SHALL PROVIDE THE RPR A WRITTEN ACCIDENT AND FIRE PROTECTION PLAN FOR ALL WORK CONTEMPLATED UNDER THE CONTRACT AND CONDUCT SAFETY MEETINGS AS APPLICABLE OR DIRECTED BY THE OWNER OR THE RPR TO MAINTAIN A SAFE AND SECURE PROJECT SITE THROUGH THE DURATION OF THE CONTRACT. SAFETY MEETINGS SHALL BE ATTENDED BY ALL SUPERVISORS, MEETING MINUTES SHALL BE PREPARED FOR ALL SAFETY MEETINGS AND SAVED IN THE CONTRACTOR'S FIELD OFFICE AND BE MADE AVAILABLE TO THE OWNER OR RPR UPON REQUEST.
4. AT THE OWNERS DISCRETION, A DAILY PROJECT SAFETY MEETING WILL BE HELD BETWEEN THE CONTRACTOR'S CSSO AND THE RPR TO REVIEW THE SAFETY REQUIREMENTS FOR THE NEXT WORK PERIOD.
5. THE CONTRACTOR SHALL PREPARE A SAFETY PLAN SPECIFIC TO NIGHTTIME CONSTRUCTION OPERATIONS IF THE CONTRACTOR ELECTS TO PERFORM CONSTRUCTION ACTIVITIES AT NIGHT.
6. THE CONTRACTOR SHALL EXERCISE EXTREME CAUTION WORKING AROUND POTENTIALLY ENERGIZED UNDERGROUND OR ABOVE GROUND ELECTRICAL UTILITIES AS WELL AS ANY AIRCRAFT FUELING EQUIPMENT. THE CONTRACTOR SHALL COORDINATE WITH THE OWNER TO IMPLEMENT A LOCK-OUT/TAG-OUT PROCEDURE FOR PERFORMANCE OF ANY WORK ON AIRPORT ELECTRICAL CIRCUITS OR FUELING EQUIPMENT. ANY UNINTENDED IMPACT TO ANY UTILITY, UTILITY LINE, FUEL LINE, OR FUEL EQUIPMENT SHALL BE REPORTED TO THE RPR IMMEDIATELY AND THE CONTRACTOR SHALL REPAIR ANY UNINTENDED IMPACT AT THEIR OWN EXPENSE .
7. CONTRACTOR SHALL RESTRICT MOVEMENT OF CONSTRUCTION VEHICLES TO CONSTRUCTION AREAS BY FLAGGING AND BARRICADING, ERECTING TEMPORARY FENCING, OR PROVIDING ESCORTS, AS APPROPRIATE. SEE SHEET G004 FOR ADDITIONAL STAGING AND HAUL ROUTE NOTES
8. CONTRACTOR SHALL ENSURE THAT NO EMPLOYEES, EMPLOYEES OF SUBCONTRACTORS OR SUPPLIERS, OR OTHER PERSONS ENTER ANY PART OF THE AIR OPERATIONS AREAS (AOA) FROM THE CONSTRUCTION SITE UNLESS AUTHORIZED BY THE RPR IN WRITING.
9. CONTRACTOR EMPLOYEES SHALL PARK AND SERVICE ALL CONSTRUCTION VEHICLES IN THE CONTRACTOR'S STAGING AREA WHEN NOT IN USE BY CONSTRUCTION PERSONNEL. (E.G., OVERNIGHT, ON WEEKENDS, OR DURING OTHER PERIODS WHEN CONSTRUCTION IS NOT ACTIVE), UNLESS SPECIFICALLY AUTHORIZED OTHERWISE IN WRITING BY THE OWNER OR RPR.
10. CONTRACTOR PERSONNEL ENGAGED IN ACTIVITIES INVOLVING OPERATIONS ON AIRCRAFT MOVEMENT AREAS SHALL BE EQUIPPED WITH RADIOS SUPPLIED BY THE CONTRACTOR AND SHALL MONITOR AIRCRAFT MOVEMENTS AND OBSERVE THE PROPER SAFETYU PROCEDURES FOR COMMUNICATION.
11. NO CONSTRUCTION MAY OCCUR WITHIN THE RUNWAY SAFETY AREA (RSA) OF ANY ACTIVE RUNWAY AT ANY TIME.
12. CONTRACTOR SHALL PROMINENTLY MARK OPEN TRENCHES AND EXCAVATIONS WITHIN THE CONSTRUCTION SITE WITH RED OR ORANGE FLAGS, AS APPROVED BY THE RPR, AND LIGHT THEM WITH RED LIGHTS DURING HOURS OF RESTRICTED VISIBILITY OR DARKNESS.
13. OPEN TRENCHES OR EXCAVATIONS ARE NOT PERMITTED WITHIN THE RSA OF ANY ACTIVE RUNWAYS ON THE AIRFIELD. IF A RUNWAY MUST BE OPENED BEFORE EXCAVATIONS ARE BACKFILLED, COVER THE EXCAVATIONS APPROPRIATELY. COVERINGS FOR OPEN TRENCHES OR EXCAVATIONS MUST BE OF SUFFICIENT STRENGTH TO SUPPORT THE WEIGHT OF THE HEAVIEST AIRCRAFT OPERATING ON THE RUNWAY.
14. OPEN TRENCHES OR EXCAVATIONS ARE NOT PERMITTED WITHIN TAXIWAY SAFETY AREAS (TSA). COVERINGS FOR OPEN TRENCHES OR EXCAVATIONS MUST BE SUFFICIENT STRENGTH TO SUPPORT THE WEIGHT OF THE HEAVIEST AIRCRAFT OPERATING ON THE TAXIWAY AND APRON WHEN THE PAVEMENT IS OPEN FOR OPERATIONS.
15. CONTRACTOR SHALL SEPARATE THE CONSTRUCTION SITE AND MOVEMENT AREAS SO THAT NO PART OF AN AIRCRAFT MAY ENTER BY USING LOW-PROFILE BARRICADES THAT ARE MARKED WITH DIAGONAL, ALTERNATING ORANGE AND WHITE STRIPES AND RED LIGHTS. BARRICADES MAY BE SUPPLEMENTED WITH ALTERNATING ORANGE AND WHITE FLAGS AT LEAST 3 FEET BY 3 FEET SQUARE AND MADE AND INSTALLED SO THEY ARE ALWAYS IN AN EXTENDED POSITION, PROPERLY ORIENTED, AND SECURELY FASTENED. (SEE DETAILS ON SHEET G008)
16. STOCKPILED MATERIALS AND EQUIPMENT STORAGE ARE NOT PERMITTED WITHIN THE RSA AND OFZ OF AN OPERATIONAL RUNWAY OR WITHIN THE TOFA OF AN OPERATIONAL TAXIWAY. CONTRACTOR SHALL ENSURE THAT STOCKPILED MATERIALS AND EQUIPMENT ADJACENT TO THESE AREAS ARE PROMINENTLY AND EXPLICITLY MARKED AND LIGHTED DURING HOURS OF RESTRICTED VISIBILITY OR DARKNESS. THIS INCLUDES DETERMINING AND VERIFYING THAT MATERIALS ARE STORED AT AN APPROVED LOCATION TO PREVENT FOREIGN OBJECT DAMAGE AND ATTRACTION OF WILDLIFE.
17. WASTE AND LOOSE MATERIALS, COMMONLY REFERRED TO AS FOD, ARE CAPABLE OF CAUSING DAMAGE TO AIRCRAFT LANDING GEAR, PROPELLERS, AND JET ENGINES. CONTRACTOR SHALL NOT LEAVE OR PLACE FOD ON OR NEAR ACTIVE AIRCRAFT MOVEMENT AREAS. MATERIALS TRACKED ONTO THESE AREAS MUST BE CONTINUOUSLY REMOVED DURING CONSTRUCTION. CONTRACTOR SHALL ALSO CAREFULLY CONTROL AND CONTINUOUSLY REMOVE WASTE OR LOOSE MATERIALS THAT MIGHT ATTRACT WILDLIFE.
18. CONTRACTOR MAY NOT USE OPEN-FLAME WELDING OR TORCHES UNLESS ADEQUATE FIRE SAFETY PRECAUTIONS ARE PROVIDED, AND THE RPR HAS APPROVED THEIR USE.

PROJECT AND SAFETY NOTES (CONTINUED)

19. CONTRACTOR SHALL NOT CONDUCT ANY CONSTRUCTION ACTIVITY WITHIN NAVIGATIONAL AID RESTRICTED AREAS AND CRITICAL AREAS, WITHOUT PRIOR APPROVAL FROM THE LOCAL FAA AIRWAY FACILITIES SECTOR REPRESENTATIVE. NAVIGATIONAL AIDS INCLUDE INSTRUMENT LANDING SYSTEM COMPONENTS SUCH AS THE LOCALIZER, THE GLIDESLOPE, APPROACH LIGHTS, PAPIS, ETC. SUCH RESTRICTED AREAS, IF APPLICABLE, ARE DEPICTED ON CONSTRUCTION PLANS.
20. ALL CONTRACTOR EQUIPMENT OPERATING IN THE AOA SHALL BE IDENTIFIED BY THREE-FOOT (3') SQUARE ORANGE AND WHITE FLAGS WHENEVER SUCH VEHICLE AND EQUIPMENT ARE OPERATING ON THE AOA. IN ADDITION, SUCH VEHICLES AND EQUIPMENT SHALL HAVE THE CONTRACTOR'S NAME CLEARLY AFFIXED ON EACH SIDE OF SUCH VEHICLES AND EQUIPMENT. DURING THE HOURS BETWEEN SUNSET AND SUNRISE AND AT ALL TIMES WHEN VISIBILITY IS IMPAIRED, VEHICLES AND MOBILE EQUIPMENT SHALL ALSO BE EQUIPPED WITH A REVOLVING YELLOW BEACON LIGHT MOUNTED ON THE TOP OF THE VEHICLE OR EQUIPMENT. BEACON LIGHTS SHALL PROVIDE:

a. THREE HUNDRED AND SIXTY DEGREE AZIMUTH COVERAGE. SEE FAA ADVISORY CIRCULAR AC 150/5200-33 - "HAZARDOUS WILDLIFE ATTRACTANTS ON OR NEAR AIRPORTS".

b. EFFECTIVE INTENSITY IN THE HORIZONTAL PLANE NOT LESS THAN 40 OR MORE THAN 400 CANDELAS.

c. BEAM SPREAD MEASURED TO 1/10 PEAK INTENSITY EXTENDING FROM 10 DEGREES TO 15 DEGREES ABOVE THE HORIZONTAL PLANE.

d. SIXTY TO NINETY FLASHES PER MINUTE.
21. NO CRANE SHALL BE ALLOWED ON THE WORK SITE UNTIL THE EQUIPMENT AND ITS INTENDED OPERATION ARE APPROVED BY THE RPR. THE CONTRACTOR SHALL PROVIDE THE RPR WITH NOT LESS THAN 72-HOUR ADVANCE WRITTEN NOTICE REQUESTING CRANE ACCESS TO THE AOA. WHEN ACCESS IS APPROVED BY THE RPR, THE TIP OF THE CRANE BOOM SHALL BE IDENTIFIED BY THE ORANGE AND WHITE FLAG AND BY RED OBSTRUCTION LIGHTS, CONFORMING TO FAA REQUIREMENTS.
22. DURING PERIODS OF SEVERE WEATHER CONDITIONS OR OTHER OPERATIONAL EMERGENCIES, THE RPR MAY DIRECT THE CONTRACTOR TO RELINQUISH AREAS UNDER CONSTRUCTION AND TO PREPARE THE AREAS FOR AIRCRAFT OPERATIONS. IN THIS EVENT THE RPR WILL SO DIRECT THE CONTRACTOR TO EVACUATE THE AREA AND WILL SPECIFY THE LIMITS OF THE AREA TO BE EVACUATED, THE TERM OF EVACUATION AND THE CONDITIONS GOVERNING THE RESTORATION WORK NECESSARY TO PREPARE THE AREA FOR AIRCRAFT OPERATION. THE CONTRACTOR SHALL PROMPTLY AND FULLY COMPLY WITH THE RPR'S DIRECTIVE. SHOULD THE DIRECTIVE ENTAIL EXTRA WORK UNDER THE CONTRACT, AS DETERMINED BY THE OWNER, THE CONTRACTOR MAY SUBMIT DOCUMENTATION SUPPORTING A REQUEST TO BE REIMBURSED FOR SUCH EXTRA WORK FOR CONSIDERATION AND ACTION BY THE OWNER. SHOULD THE DIRECTIVE ENTAIL A DELAY IN THE COMPLETION OF THE CONTRACT OR ANY DEFINED SUBDIVISION OF THE CONTRACT, AS DETERMINED BY THE OWNER, THE CONTRACTOR MAY SUBMIT DOCUMENTATION SUPPORTING A REQUEST TO BE GRANTED AN EXTENSION OF TIME FOR CONSIDERATION AND ACTION BY THE OWNER.
23. VEHICULAR TRAFFIC SHALL NOT CROSS ACTIVE AIRCRAFT MOVEMENT AREAS (RUNWAYS, TAXIWAYS OR AIRCRAFT PARKING APRON) WITHOUT PRIOR APPROVAL OF AIRPORT MANAGEMENT/OPERATIONS. ANY CONSTRUCTION ACTIVITIES IN OR ADJACENT TO ACTIVE AIRCRAFT MOVEMENT AREAS SHALL BE PROPERTY MARKED, BARRICADED, AND SECURED.
24. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE ACTIONS OF EMPLOYEES AND SUBCONTRACTORS. PERSONNEL WHO DO NOT ABIDE BY AIRPORT RULES AND REGULATIONS ARE SUBJECT TO PROSECUTION, REMOVAL FROM THE AIRPORT, AND/OR MONETARY FINES.
25. ALL ACCIDENTS CAUSING PERSONAL INJURY OR PROPERTY DAMAGE SHALL BE REPORTED IMMEDIATELY TO THE RPR.
26. THE CONTRACTOR(S) SHALL PROVIDE, AT THE SITE, SUCH EQUIPMENT AND MEDICAL FACILITIES AS ARE NECESSARY TO SUPPLY FIRST AID SERVICE TO ANYONE WHO MAY BE INJURED IN CONNECTION WITH THE PERFORMANCE OF THE WORK, WHETHER ON OR ADJACENT TO THE SITE. IN ADDITION, IF DEATH OR SERIOUS INJURIES OR SERIOUS DAMAGES ARE CAUSED, THE ACCIDENT SHALL BE REPORTED IMMEDIATELY BY TELEPHONE TO 911 DISPATCH.
27. THE CONTRACTOR'S EMPLOYEES MUST HAVE A VALID GOVERNMENTAL IDENTIFICATION ON THEIR PERSON AT ALL TIMES. FAILURE TO COMPLY WITH THESE REQUIREMENTS WILL RESULT IN THE EMPLOYEE BEING ESCORTED OFF THE AOA AND FINES MAY BE IMPOSED AT THE CONTRACTOR'S EXPENSE.
28. ANY DELAY IN CONSTRUCTION OF PROJECT DUE TO VIOLATION OF FEDERAL AND/OR AIRPORT REGULATIONS SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR.
29. CONTRACTOR WILL NOT BE ALLOWED TO USE ANY OF THE EXISTING TAXIWAYS OR APRONS OR OTHER PAVEMENT AREA NOT AFFECTED BY THE CONSTRUCTION FOR ANY PURPOSE UNLESS SPECIFICALLY AUTHORIZED BY THE OWNER OR RPR.
30. CONTRACTOR IS REQUIRED TO HAVE A PERSON ON CALL 24-HOURS A DAY, 7 DAYS A WEEK FOR EMERGENCY MAINTENANCE OF AIRPORT HAZARD LIGHTING AND BARRICADES. CONTRACTOR MUST FILE THE CONTACT PERSON'S NAME WITH THE OWNER OR RPR. ALL CHANGES IN PERSONNEL MUST BE REPORTED TO THE RPR WITHIN 24 HOURS.
31. CONTRACTOR SHALL PROVIDE AN EMERGENCY NOTIFICATION LIST (24-HOUR CONTACT LIST) WHICH IDENTIFIES THE CONTRACTOR'S PROJECT SUPERVISORY STAFF, AND ALL SUBCONTRACTOR SUPERVISORS/LEADS. ALL CHANGES IN PERSONNEL MUST BE REPORTED TO THE RPR WITHIN 24 HOURS.
32. ANY TAXIWAY AND RUNWAY CLOSURE REQUESTS, OUTSIDE OF THAT SPECIFIED WITHIN CONTRACT DOCUMENTS, MUST BE SUBMITTED 72 HRS IN ADVANCE OF THE REQUESTED CLOSURE PERIOD. THE REQUEST MUST CONTAIN A DRAWING THAT CLEARLY DEPICTS THE AREA(S) TO BE CLOSED AND A SCHEDULE OF OPERATIONS AND TIME(S) REQUIRED FOR CONSTRUCTION.
33. ALL TAXIWAY AND RUNWAY CLOSURE REQUESTS ARE SUBJECT TO THE REVIEW AND APPROVAL OF THE OWNER OR RPR, AS APPLICABLE. THE OWNER RESERVES THE RIGHT TO SHIFT ANY APPROVED CLOSURE PERIODS TO ADDRESS AIRCRAFT AND AIRPORT OPERATIONAL REQUIREMENTS.

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FBO/TERMINAL APRON  
REHABILITATION  
CITY OF AVON PARK  
AVON PARK, FL

**Kimley»Horn**  
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SAFETY NOTES

G006

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SECURITY NOTES

1. IT IS INTENDED THAT THE CONTRACTOR SHALL COMPLY WITH ALL SECURITY REQUIREMENTS SPECIFIED HEREIN AND IN THE CONTRACT DOCUMENTS. THE CONTRACTOR SHALL DESIGNATE TO THE OWNER AND RPR IN WRITING THE NAME OF ITS "CONTRACTOR SECURITY SAFETY OFFICER" (CSSO). THE CSSO SHALL REPRESENT THE CONTRACTOR ON ALL SECURITY REQUIREMENTS OF THE CONTRACT. THE CSSO FOR THIS PROJECT MAY BE THE PROJECT SUPERINTENDENT/SUPERVISOR. THE CSSO SHALL BE REQUIRED TO FAMILIARIZE THEMSELVES WITH REQUIREMENTS OF OPERATING WITHIN THE AIRFIELD AND APPLICABLE RULES AND REGULATIONS.
2. THE CONTRACTOR SHALL MAINTAIN AND SUBMIT A CONTINUOUSLY UPDATED LIST OF ALL AUTHORIZED PROJECT PERSONNEL TO OWNER AND RPR PRIOR TO BEING AUTHORIZED TO BEGIN WORK ON THE PROJECT SITE. THE CSSO SHALL BE RESPONSIBLE FOR BRIEFING ALL CONTRACTOR PERSONNEL ON THESE REQUIREMENTS AND, FROM TIME TO TIME, OTHER SECURITY PROVISIONS ADOPTED BY THE AIRPORT. ALL NEW CONTRACTOR EMPLOYEES SHALL BE BRIEFED ON THESE REQUIREMENTS PRIOR TO WORKING IN THE CONSTRUCTION AREA.
3. ALL EMPLOYEES, AGENTS, VENDORS, INVITEES, ETC. OF THE CONTRACTOR OR SUBCONTRACTORS HAVING ACCESS TO THE AOA SHALL CONSPICUOUSLY WEAR EITHER SHIRT AND/OR HARD HAT THAT IDENTIFIES THE PERSON TO THE PROJECT AND EMPLOYER.
4. IDENTIFICATION OF VEHICLES: THE CONTRACTOR SHALL ESTABLISH AND MAINTAIN A LIST OF CONTRACTOR AND SUB-CONTRACTOR VEHICLES AUTHORIZED TO OPERATE ON THE SITE. VEHICLE PERMITS SHALL BE ASSIGNED IN A MANNER IN WHICH TO ASSURE POSITIVE IDENTIFICATION OF THE UNIT AT ALL TIMES. IN LIEU OF ISSUING INDIVIDUAL PERMITS, THE CSSO CAN REQUIRE EACH VEHICLE TO DISPLAY A LARGE COMPANY SIGN ON BOTH SIDES OF THE VEHICLE AND ISSUE TO THE RPR, A CURRENT LIST OF COMPANIES AUTHORIZED TO ENTER AND CONDUCT WORK ON THE AIRPORT. CONTRACTOR EMPLOYEE PERSONAL VEHICLES ARE NOT ALLOWED ON THE AIRFIELD AT ANY TIME.
5. THE CONTRACTOR'S ACCESS TO THE SITE SHALL BE AS SHOWN ON THE PLANS OR AS DIRECTED BY THE RPR IN WRITING. NO OTHER ACCESS POINTS SHALL BE ALLOWED UNLESS APPROVED BY THE OWNER AND/OR DIRECTED BY THE RPR. ALL CONTRACTOR TRAFFIC AUTHORIZED TO ENTER THE SITE SHALL BE FAMILIAR WITH THE ROUTE OR GUIDED BY CONTRACTOR PERSONNEL FAMILIAR WITH THE CONSTRUCTION PROJECT SITE. THE CONTRACTOR SHALL BE RESPONSIBLE FOR TRAFFIC CONTROL TO AND FROM THE VARIOUS CONSTRUCTION AREAS ON AIRPORT PROPERTY. DIRECTIONAL SIGNING AT THE ACCESS POINT AND ALONG THE DELIVERY ROUTE TO THE STORAGE AREA OR WORK SITE SHALL BE AS APPROVED BY THE RPR.
6. ALL MATERIAL ORDERS FOR DELIVERY TO THE PROJECT SITE MUST BE ESCORTED BY THE CONTRACTOR. THIS WILL PRECLUDE DELIVERY TRUCKS FROM UNCONTROLLED ENTRY INTO THE AIRPORT OR ENTERING INTO AN AIRPORT OPERATIONS AREA INADVERTENTLY.
7. THE MATERIAL STORAGE AREA, EQUIPMENT STORAGE AREA, PARKING AREA AND OTHER AREAS DEFINED AS REQUIRED FOR THE CONTRACTOR'S EXCLUSIVE USE DURING CONSTRUCTION SHALL BE AS SHOWN ON PLANS OR AS APPROVED BY THE RPR IN WRITING. THE CONTRACTOR SHALL ERECT, MAINTAIN AND SHALL REMOVE SUITABLE FENCING, MARKING AND/OR WARNING DEVICES FOR DAY/NIGHT USE AROUND THE PERIMETER OF THE AREA.
8. THE CONTRACTOR ACCESS GATE DESIGNATED FOR USE MAY BE UTILIZED BY OTHER CONTRACTORS OR BY AIRPORT STAFF DURING THIS PROJECT. THE CONTRACTOR IS REQUIRED TO COORDINATE ACCESS WITH ALL PARTIES AUTHORIZED TO ENTER THE AIRPORT. NO ADDITIONAL COMPENSATION SHALL BE MADE FOR ISSUES RELATED TO SITE ACCESS.
9. THE CONTRACTOR SHALL INSTALL ITS OWN LOCK AT EACH EXISTING AIRPORT ACCESS GATE AUTHORIZED FOR USE IN THIS CONTRACT. THE CONTRACTOR SHALL INSTALL ITS LOCK BY INTERLOCKING TO THE EXISTING AIRPORT LOCK ON THE GATE. THE CONTRACTOR SHALL PROVIDE FOUR (4) DUPLICATE KEYS FOR EACH LOCK TO THE RPR. LOCK ID TAGS SHALL BE PLACED ON EACH LOCK BY THE CONTRACTOR WITH THE COMPANY NAME AND EMERGENCY CONTACT NUMBER INSCRIBED ON THE SURFACE.
10. IT IS THE CONTRACTOR'S RESPONSIBILITY TO KEEP THE AIRPORT SECURED AT ALL TIMES DURING THE DURATION OF THE PROJECT. ALL ACCESS GATES SHALL BE CLOSED AND LOCKED AT ALL TIMES, UNLESS THE CONTRACTOR PROVIDES A FULL-TIME BONDED SECURITY GATE GUARD WHO IS TRAINED AND FOR ACCESS TO THE AOA.
11. NO GATE IS TO BE LEFT UNATTENDED AT ANY TIME. GATES ARE TO BE CLOSED AND LOCKED WHEN NOT IN USE.

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FBO/TERMINAL APRON  
REHABILITATION  
CITY OF AVON PARK  
AVON PARK, FL

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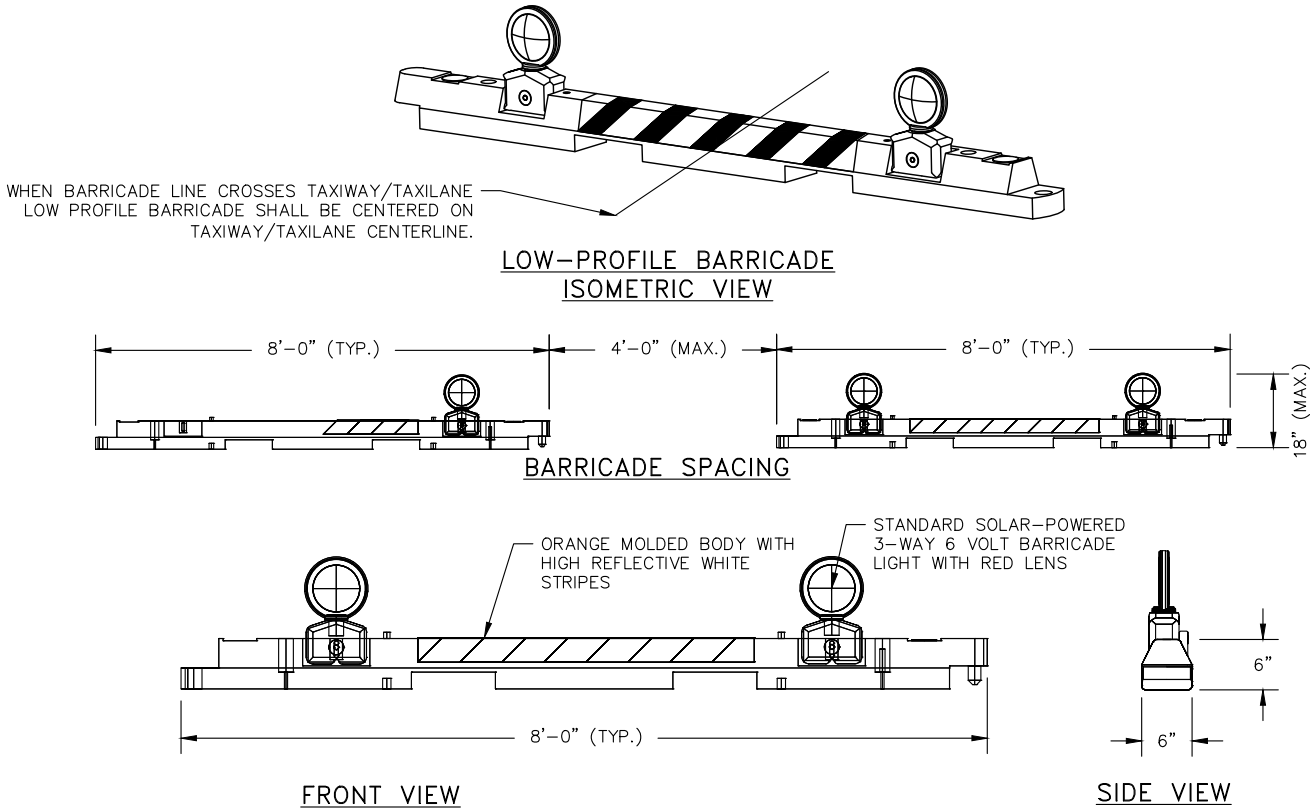
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SECURITY NOTES

G007



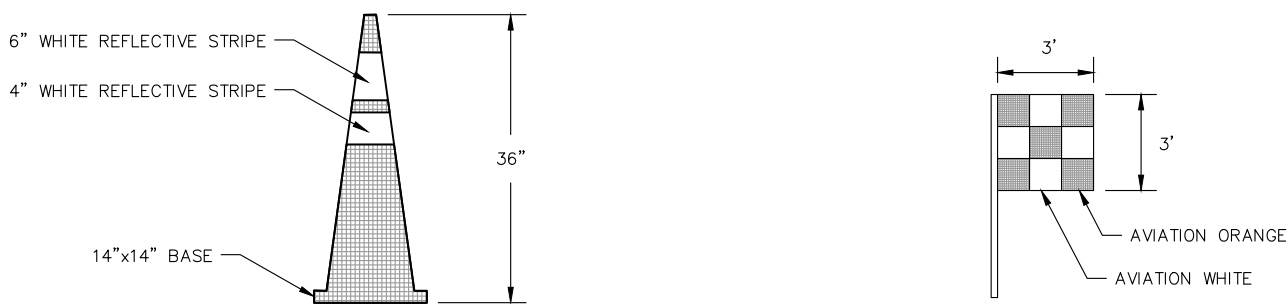
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**1 TYPE 1 LOW-PROFILE BARRICADE DETAIL**  
G008 NOT TO SCALE

**LOW PROFILE BARRICADE NOTES:**

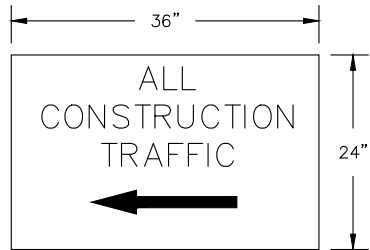
1. MAXIMUM TOTAL HEIGHT OF BARRICADES SHALL BE 18 INCHES.
2. TYPICAL BARRICADE SHALL BE PLACED, ALONG THE LIMITS OF THE PHASES OF WORK, AS SHOWN IN THESE PLANS TO DELINEATE THE CONTRACTOR'S WORK AREAS AND CLOSED AIRFIELD PAVEMENTS.
3. BARRICADES SHALL EITHER BE WATER FILLED OR WEIGHTED WITH SANDBAGS TO SECURE AGAINST MOVEMENT. SANDBAGS SHALL BE A MINIMUM OF 25 LBS. EACH WITH ONE PLACED ON EACH END.
4. ALL BARRICADES AND SANDBAGS SHALL BE CHECKED VISUALLY FOR SIGNS OF WEAR AND TEAR ON A DAILY BASIS. BARRICADES SHALL BE REPAINTED AND SANDBAGS REPLACED WHEN DEEMED APPROPRIATE BY THE OWNER OR RPR. ALL LIGHT FIXTURES SHALL BE IN GOOD WORKING ORDER AND SHALL BE CHECKED BY THE CONTRACTOR ON A DAILY BASIS BEFORE THE CONTRACTOR CEASES OPERATIONS FOR THE DAY.
5. ALL BARRICADES SHALL BE MOVED AT LEAST ONCE EACH WEEK, OR AS DIRECTED BY THE OWNER OR RPR, AND THE CONTRACTOR SHALL SWEEP THE ACCUMULATED DEBRIS AND REMOVE AND DISPOSE OF THE DEBRIS OFFSITE AT AN APPROVED DISPOSAL FACILITY. THE BARRICADES SHALL THEN BE REPLACED AT THE APPROPRIATE LOCATION.
6. BARRICADE LIGHTS SHALL BE RED IN COLOR AND FLASHING DURING HOURS OF LOW VISIBILITY AND DARKNESS.
7. ALL LOW PROFILE BARRICADES ARE TO BE PLACED AT A MAXIMUM INTERVAL OF 4 FEET END TO END.
8. THE CONTRACTOR MAY SUBSTITUTE AN EQUAL LOW PROFILE BARRICADE FOR APPROVAL BY THE ENGINEER, GENERALLY CONFORMING TO THE ABOVE DETAILS, AND CONFORMING WITH THE SPECIFICATIONS.
9. COST OF FURNISHING AND MAINTAINING BARRICADES SHALL BE INCLUDED IN THE CONTRACTOR'S BID UNIT COST FOR ITEM M-102 MAINTENANCE OF TRAFFIC AND TEMPORARY CONSTRUCTION ITEMS.



NOTE: TRAFFIC CONES SHALL BE PLACED AT A MAXIMUM INTERVAL OF 10 FEET ALONG ON-PAVEMENT HAUL ROUTES OR AS REQUIRED BY THE RPR.

**2 ORANGE TRAFFIC CONE DETAIL**  
G008 NOT TO SCALE

**3 VEHICLE FLAG DETAIL**  
G008 NOT TO SCALE



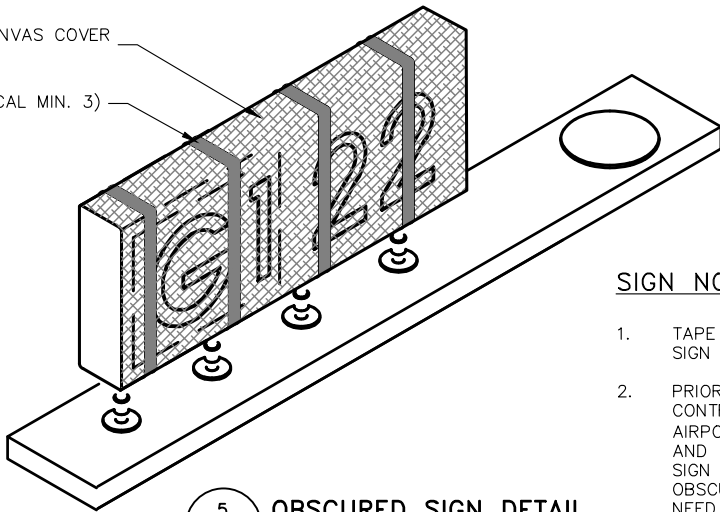
**SIGN NOTES**

1. SIGN BACKGROUND IS TO BE WHITE. SIGN LETTERING IS TO BE BLACK, MINIMUM HEIGHT OF 3", AND BE A BOLD LETTERING STYLE SIMILAR TO THE THE DETAIL SHOWN.
2. SIGN IS TO BE LOCATED HIGH ENOUGH TO ALLOW EASY VIEWING FROM ALL THE CONSTRUCTION VEHICLES ENTERING THE CONSTRUCTION SITE.
3. SIGN PLACEMENT SHALL BE AS APPROVED BY THE OWNER OR RPR.
4. ALL TRAFFIC SIGNAGE SHALL BE INCLUDED AND IS INCIDENTAL TO PAY ITEM M-102.

**4 CONSTRUCTION TRAFFIC SIGN**  
G008 NOT TO SCALE

1/4" MASONITE OR HEAVY CANVAS COVER OR APPROVED EQUAL

1" CAM BUCKLE STRAP (TYPICAL MIN. 3)



**SIGN NOTES**

1. TAPE SHALL NOT BE USED TO SECURE SIGN COVERS.
2. PRIOR TO OBSCURING SIGN, THE CONTRACTOR SHALL COORDINATE WITH AIRPORT MANAGEMENT/OPERATIONS AND THE RPR TO DETERMINE SPECIFIC SIGN PANELS THAT NEED TO BE OBSCURED, AND SIGN PANELS THAT NEED TO REMAIN VISIBLE.

**5 OBSCURED SIGN DETAIL**  
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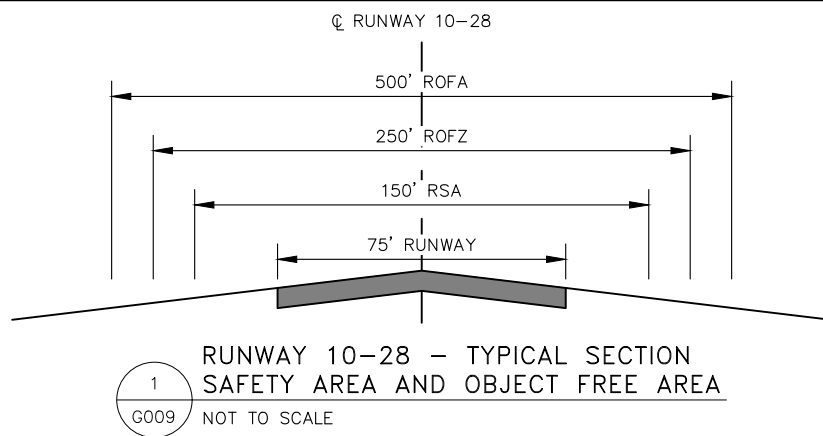
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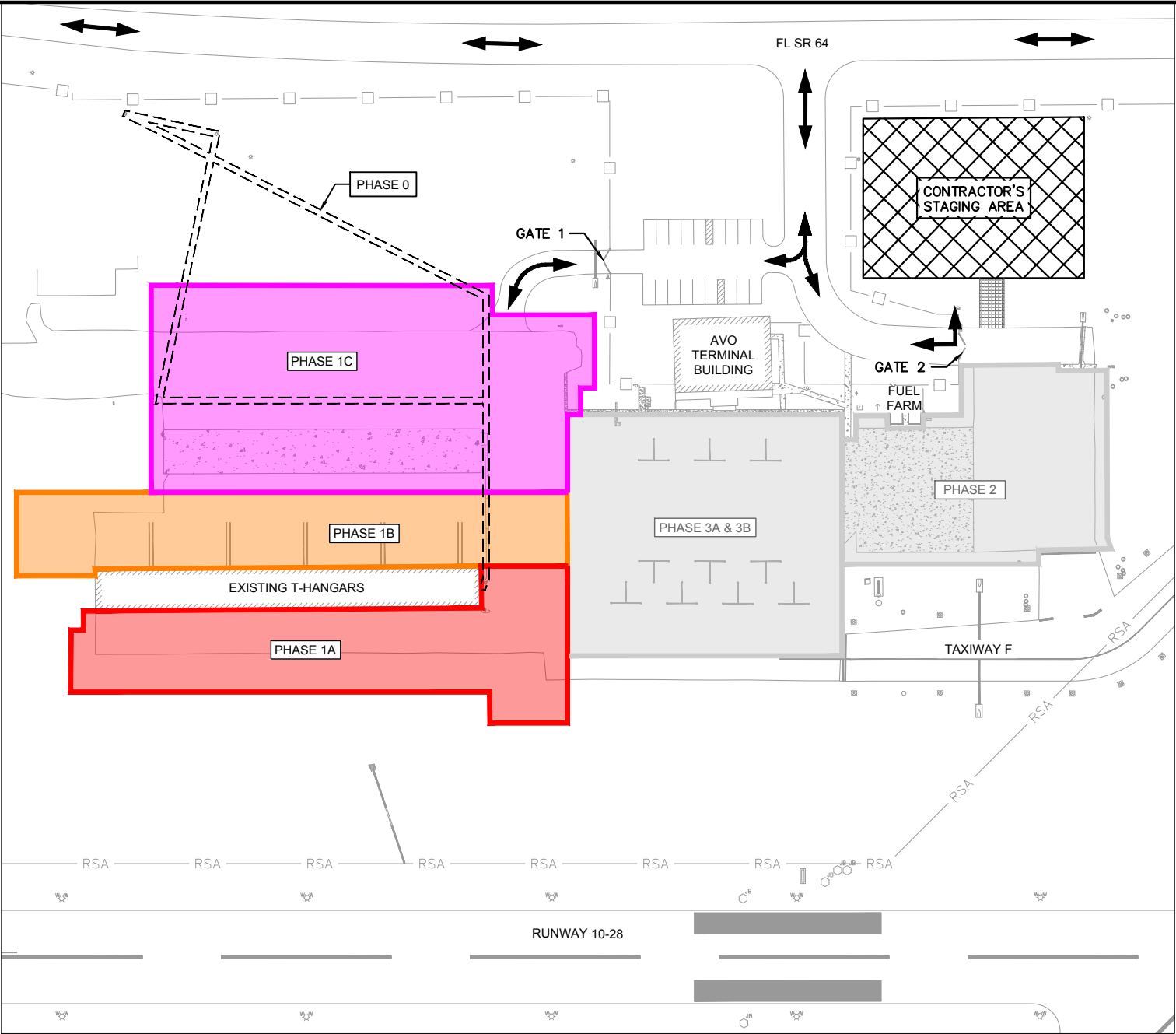
SAFETY AND MOT DETAILS

**G008**

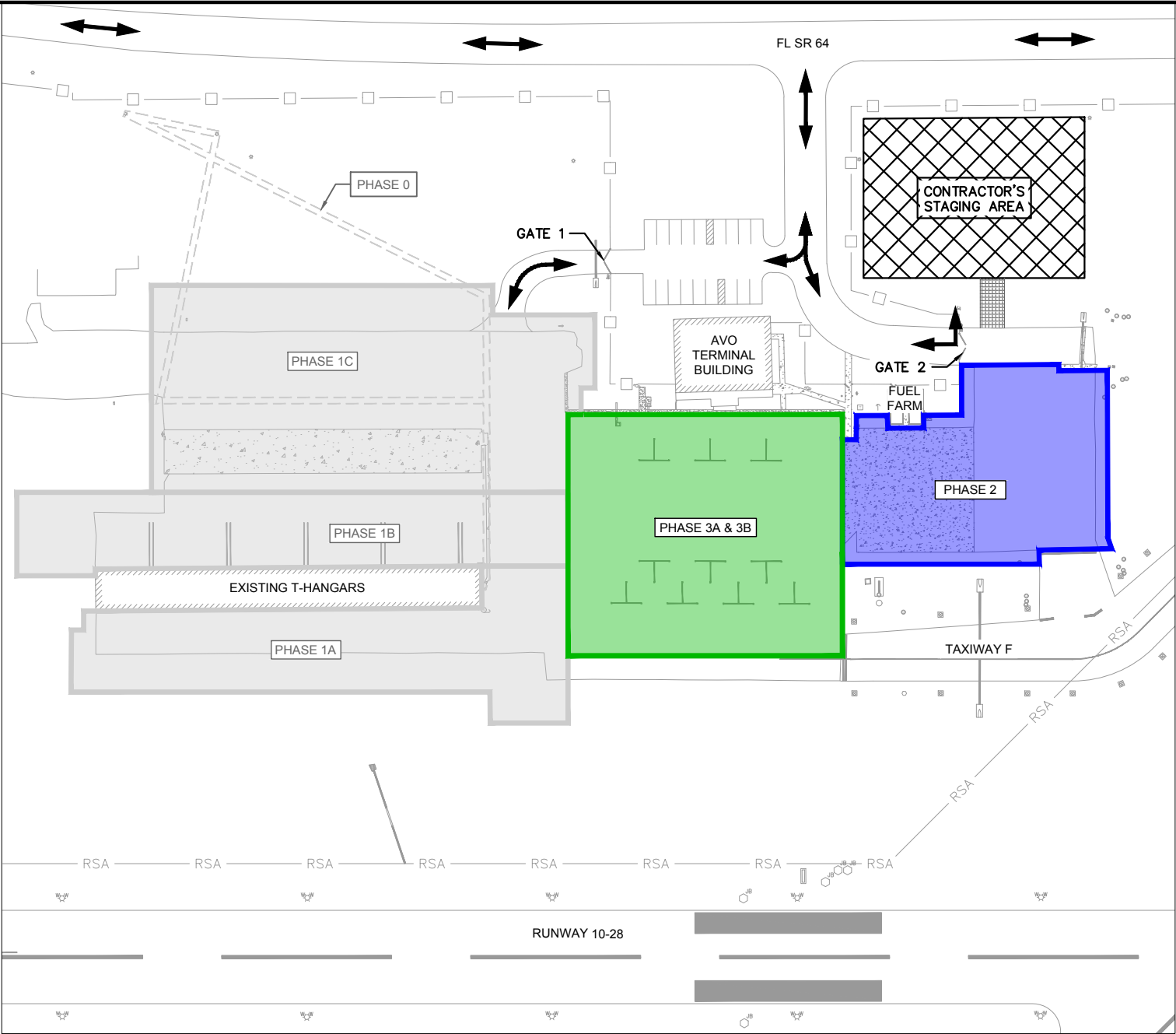
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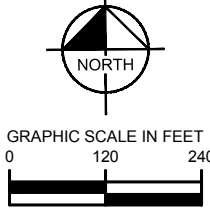
PHASE 0, 1A, 1B, & 1C



PHASE 2, 3A, & 3B

GENERAL PHASING NOTES:

1. PHASING SHOWN IS BASED ON AVON PARK EXECUTIVE AIRPORT'S (AVO) OPERATIONAL REQUIREMENTS. CONTRACTOR TO SUPPLY DETAILED CONSTRUCTION PHASING PLAN, SCHEDULE, AND SAFETY PLAN COMPLIANCE DOCUMENT (SPCD) BEFORE ANY CONSTRUCTION IS PERMITTED.
2. PRIOR TO THE COMMENCEMENT OF EACH PHASE, THE CONTRACTOR SHALL REVIEW THE PROPOSED LOCATION OF BARRICADES AND MOT DEVICES WITH THE RPR AND AIRPORT.
3. A MINIMUM OF TWO-WEEKS PRIOR TO CLOSURE OF ANY AIRFIELD FACILITY THE CONTRACTOR SHALL COORDINATE WITH THE AIRPORT FOR TEMPORARY COVERING OR DECOMMISSIONING OF AIRFIELD LIGHTING, SIGNAGE, AND NAVIGATIONAL AIDS.
4. THE OWNER RESERVES THE RIGHT TO RE-SEQUENCE WORK AREAS WITHIN THE SPECIFIED PHASING LIMITS AT NO CHANGE TO CONTRACT TIME AND NO COST TO THE OWNER TO ACCOMMODATE AIRPORT OPERATIONAL REQUIREMENTS
5. CONTRACTOR TO PERFORM SITE INVESTIGATION AND LOCATE ALL UTILITIES, INCLUDING AIRFIELD ELECTRICAL AND COMMUNICATIONS INFRASTRUCTURE, PRIOR TO ENTERING EACH PHASE OF WORK. CONTRACTOR TO COORDINATE WITH AIRPORT FOR ACCESS A MINIMUM OF TWO WEEKS PRIOR TO ENTERING EACH SUBSEQUENT WORK AREA.
6. DESCRIPTIONS OF WORK LISTED WITHIN THESE PHASING PLANS ARE INTENDED TO PROVIDE GENERAL GUIDANCE ON THE SCOPE OF WORK WITHIN EACH WORK AREA AND ARE NOT TO BE CONSIDERED EXHAUSTIVE OF ALL WORK REQUIREMENTS. SPECIFIC WORK ELEMENTS SHOWN IN OTHER SECTIONS OF THE PLANS SHALL REPRESENT THE CONTRACT SCOPE OF WORK.



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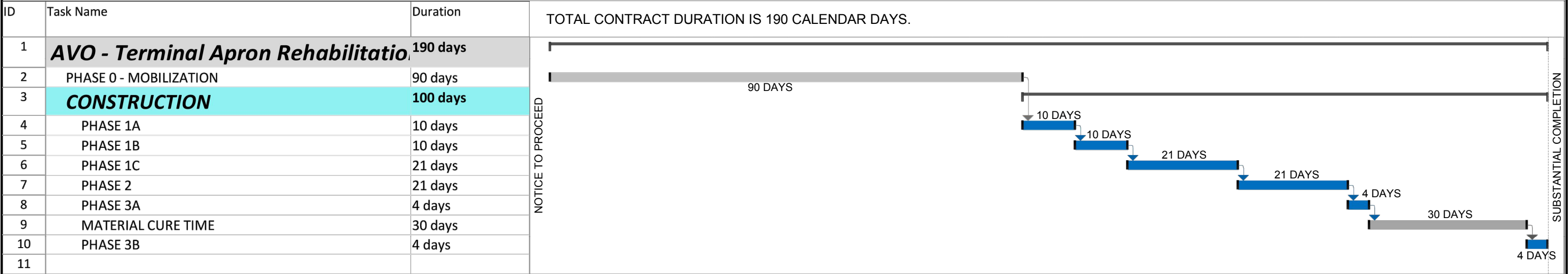
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OVERALL PHASING PLAN

P110

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Avon Park Executive Airport (AVO)  
FBO/Terminal Rehabilitation  
Estimated Construction Schedule



NOTE: PHASE DURATIONS SHOWN ABOVE ARE EXPRESSED IN CONTRACT CALENDAR DAYS.

PHASE 0 – CONTRACTOR’S MOBILIZATION, HAUL ROUTE SIGNAGE, EQUIPMENT PROCUREMENT, PROJECT SCHEDULE, CONTRACTOR’S QUALITY CONTROL AND SAFETY PLAN, SAFETY PLAN COMPLIANCE DOCUMENT, CONTRACTOR’S STAGING AREA, CONSTRUCTION PERMITTING BY THE CONTRACTOR, PERMITTING AND INSTALLATION OF BATCH PLANT (IF APPLICABLE), SUBMITTALS, ORDERING OF LONG-LEAD ITEMS, ETC. THIS PHASE INCLUDES CONTRACTOR’S COORDINATION WITH DUKE ENERGY AND RPR REGARDING ALL ELECTRICAL WORK. CONTRACTOR TO FIELD VERIFY LOCATION AND DEPTH OF ELECTRICAL SERVICE AND OTHER UTILITIES IN THE PROJECT AREA. CONSTRUCT NEW ELECTRICAL SERVICE WITH TEMPORARY BACKFILL IN PAVED AREAS AND LAYOUT NEW ELECTRICAL SERVICE FROM EXISTING T-HANGAR TO POWER POLE IN COORDINATION WITH DUKE ENERGY AND DUKE ENERGY STANDARDS.

PHASE 1A – THIS PHASE INCLUDES ALL WORK REQUIRED TO COMPLETE THE DEMOLITION OF THE ASPHALT APRON SHOWN IN PHASE 1A, RECONSTRUCT THE ASPHALT APRON AND TURF GRADING IN THE AREA SOUTH AND ADJACENT TO THE EXISTING T-HANGAR. THE NORTH SIDE OF THE EXISTING T-HANGAR SHALL REMAIN OPEN DURING THIS PHASE.

PHASE 1B – THIS PHASE INCLUDES ALL WORK REQUIRED TO COMPLETE THE DEMOLITION OF THE ASPHALT APRON SHOWN IN PHASE 1B, RECONSTRUCT THE ASPHALT APRON AND TURF GRADING IN THE AREA NORTH AND ADJACENT TO THE EXISTING T-HANGAR. THE SOUTH SIDE OF THE EXISTING T-HANGAR SHALL REMAIN OPEN DURING THIS PHASE.

PHASE 1C – THIS PHASE INCLUDES ALL WORK REQUIRED TO COMPLETE THE DEMOLITION OF THE ASPHALT APRON AND CONCRETE SLAB SHOWN IN PHASE 1C, RECONSTRUCT THE ASPHALT APRON AND TURF GRADING IN THE AREA NORTH OF THE EXISTING T-HANGAR AND NORTH OF THE FBO/TERMINAL APRON. THE EXISTING T-HANGAR SHALL REMAIN FULLY OPERATIONAL DURING THIS PHASE.

PHASE 2 – THIS PHASE INCLUDES ALL WORK REQUIRED TO COMPLETE THE DEMOLITION OF THE ASPHALT APRON AND CONCRETE SLAB SHOWN IN PHASE 2, RECONSTRUCT THE ASPHALT APRON, AND GRADING IN THE AREA SOUTH AND EAST OF THE EXISTING FUEL FARM. THE TAXIWAY CONNECTOR NORTH OF TAXIWAY F SHALL UTILIZE LOW-PROFILE BARRICADES AS SHOWN ON THE PHASING PLANS. THE EAST PORTION OF THE FBO/TERMINAL APRON AND FUEL FARM SHALL BE CLOSED DURING THIS PHASE. AIRCRAFT SHALL UTILIZE TAXIWAY F FOR ACCESS TO THE FBO/TERMINAL APRON.

PHASE 3A – THIS PHASE INCLUDES ALL WORK REQUIRED TO COMPLETE THE APPLICATION OF THE PAVEMENT REJUVENATOR ON THE CENTRAL PORTION OF THE FBO/TERMINAL APRON AS SHOWN IN PHASE 3A. THE CENTRAL AND WEST PORTIONS OF THE FBO/TERMINAL APRON WILL BE CLOSED DURING THIS PHASE.

PHASE 3B – THIS PHASE INCLUDES ALL WORK REQUIRED TO COMPLETE THE APPLICATION OF FINAL PAVEMENT MARKINGS ON THE CENTRAL PORTION OF THE FBO/TERMINAL APRON AS SHOWN IN PHASE 3B. THE CENTRAL AND WEST PORTIONS OF THE FBO/TERMINAL APRON WILL BE CLOSED DURING THIS PHASE.

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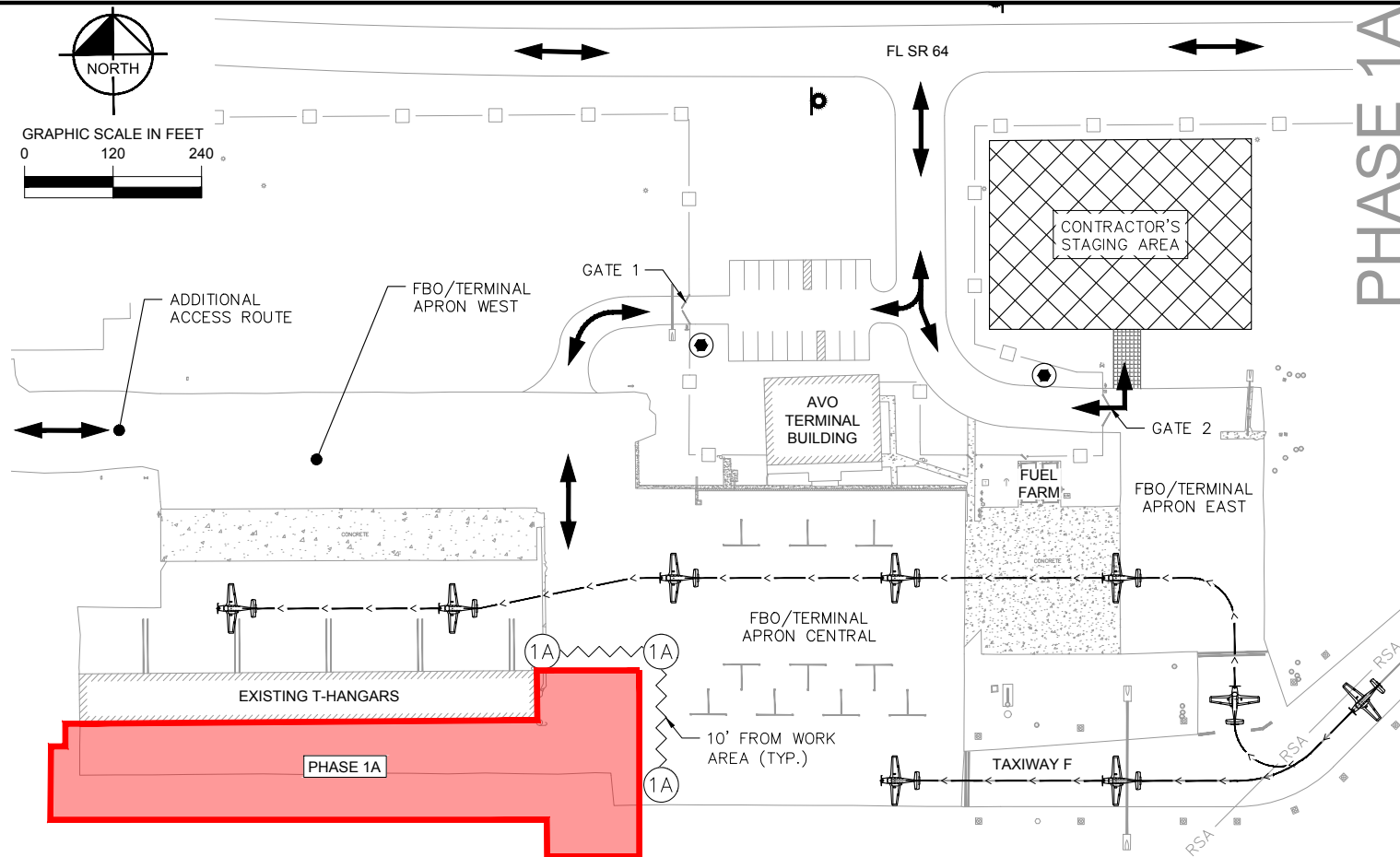
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PROJECT SCHEDULE

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**PHASE 0 MOBILIZATION:**

1. CONTRACTOR TO ESTABLISH ALL MOT DEVICES AND HAUL ROUTE SIGNAGE, PROCURE EQUIPMENT, PRODUCE PROJECT SCHEDULE, CONTRACTOR'S QUALITY CONTROL AND SAFETY PLAN, SAFETY PLAN COMPLIANCE DOCUMENT, CONSTRUCT CONTRACTOR'S STAGING AREA, OBTAIN ALL CONSTRUCTION PERMITTING, SUBMIT SUBMITTALS, ORDER LONG-LEAD ITEMS.
2. CONTRACTOR TO FIELD VERIFY LOCATION AND DEPTH OF ELECTRICAL SERVICE AND ANY OTHER UTILITIES IN THE PROJECT AREA PER ITEM L-100-3.
3. CONTRACTOR TO COORDINATE WITH DUKE ENERGY AND RPR REGARDING ALL PROJECT ELECTRICAL WORK. CONSTRUCT INTERIM ELECTRICAL SERVICE IN PAVED AREAS AND LAYOUT NEW ELECTRICAL SERVICE FROM EXISTING T-HANGAR TO POWER POLE IN COORDINATION WITH DUKE ENERGY AND DUKE ENERGY STANDARDS.
4. CONTRACTOR TO DEMOLISH EXISTING ELECTRICAL SERVICE AS INDICATED ON SHEET E110 AND INSTALL NEW ELECTRICAL SERVICE WITH TEMPORARY BACKFILL AS SHOWN ON SHEET E210 AND DETAIL 1, SHEET E290.
5. FINAL BACKFILL FOR CONCRETE ENCASED ELECTRICAL SERVICE SECTION AS SHOWN IN DETAIL 2, SHEET E290 SHALL BE INSTALLED WITHIN THE OUTLINED PHASE LIMITS.

**PHASE 1A DESCRIPTION:**

1. PHASE 1A TO BEGIN IMMEDIATELY FOLLOWING PHASE 0.
2. SET UP MOT, EROSION CONTROL INSTALLATION, AND CLEARING AND GRUBBING.
3. FULL DEPTH ASPHALT PAVEMENT REMOVAL OF APRON PAVEMENT WITHIN SPECIFIED LIMITS.
4. CONSTRUCT ASPHALT PAVEMENT.
5. TURF AND DRAINAGE GRADING.
6. TOPSOIL AND SODDING.

**PHASE 1A AIRCRAFT MOVEMENTS:**

1. THE FBO/TERMINAL APRON SHALL REMAIN OPERATIONAL DURING THIS PHASE.
2. THE SOUTHERN PORTION OF THE EXISTING T-HANGARS SHALL NOT BE ACCESSIBLE DURING THIS PHASE.

**PHASE 1A LIMITS OF WORK:**

1. PHASE WORK LIMITS ARE GENERALLY AS SHOWN.

**PHASE 1B DESCRIPTION:**

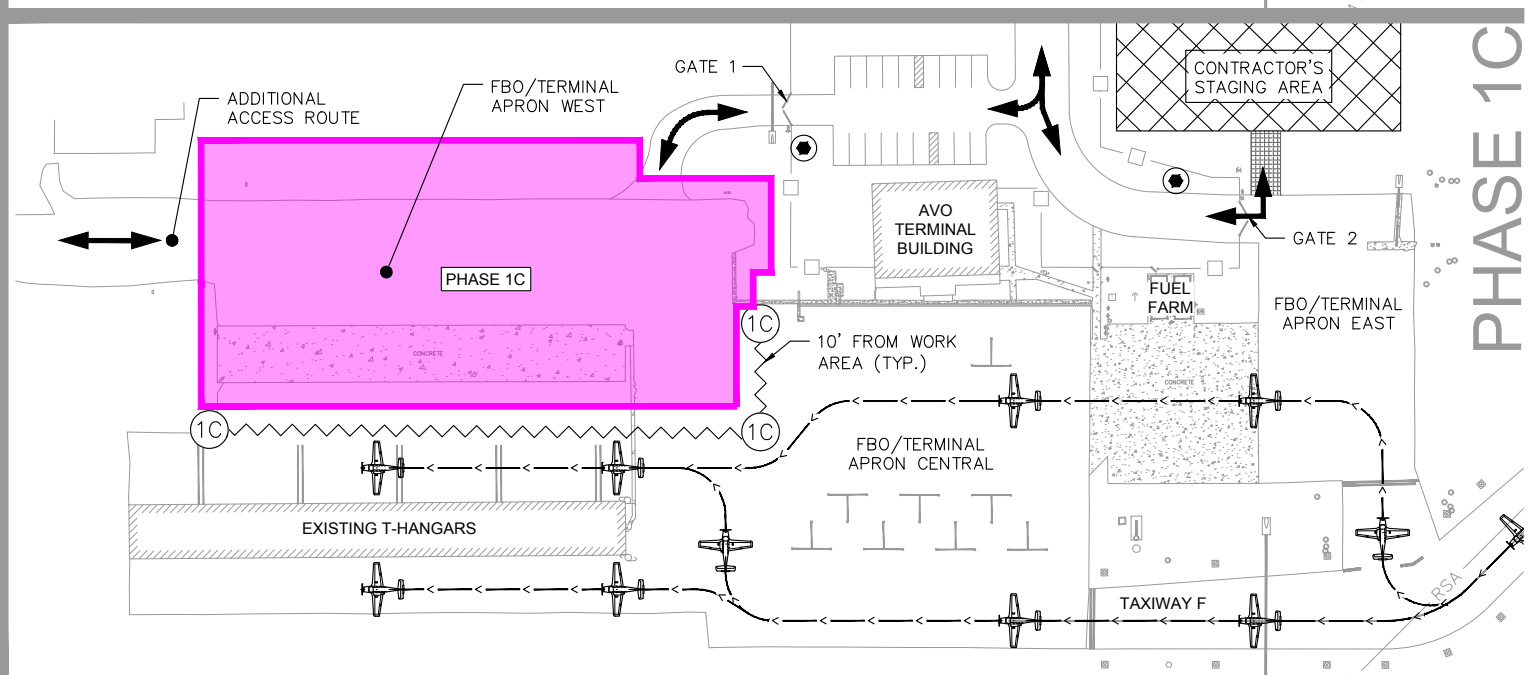
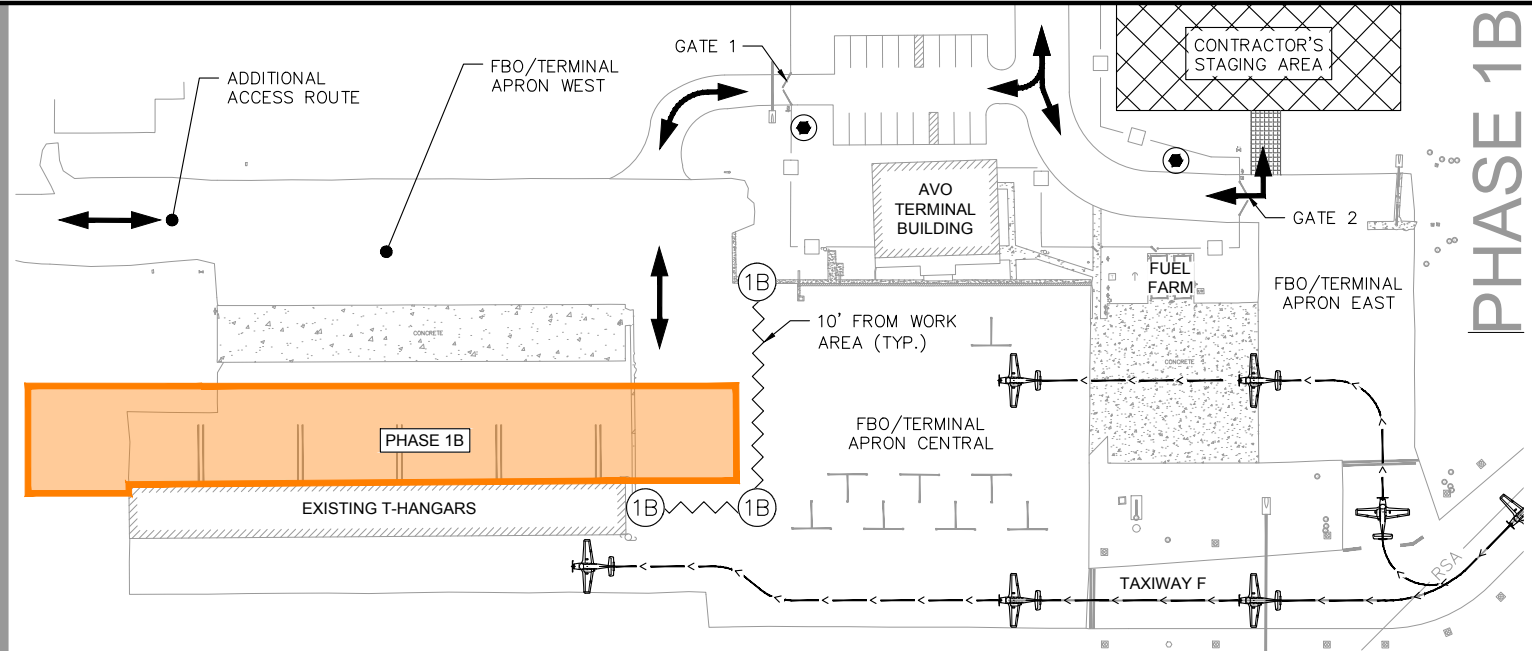
1. PHASE 1B TO BEGIN IMMEDIATELY FOLLOWING PHASE 1A.
2. SET UP MOT, EROSION CONTROL INSTALLATION, AND CLEARING AND GRUBBING.
3. FULL DEPTH ASPHALT PAVEMENT REMOVAL OF APRON PAVEMENT WITHIN SPECIFIED LIMITS.
4. CONSTRUCT ASPHALT PAVEMENT.
5. TURF AND DRAINAGE GRADING.
6. TOPSOIL AND SODDING.

**PHASE 1B AIRCRAFT MOVEMENTS:**

1. THE FBO/TERMINAL APRON SHALL REMAIN OPERATIONAL DURING THIS PHASE.
2. THE NORTHERN PORTION OF THE EXISTING T-HANGARS SHALL NOT BE ACCESSIBLE THIS PHASE.

**PHASE 1B LIMITS OF WORK:**

1. PHASE WORK LIMITS ARE GENERALLY AS SHOWN.



**PHASE 1C DESCRIPTION:**

1. PHASE 1C TO BEGIN IMMEDIATELY FOLLOWING PHASE 1B.
2. SET UP MOT, EROSION CONTROL INSTALLATION, AND CLEARING AND GRUBBING.
3. FULL DEPTH ASPHALT PAVEMENT REMOVAL OF APRON PAVEMENT AND FULL DEPTH REMOVAL ON CONCRETE SLAB WITHIN THE SPECIFIED LIMITS.
4. CONSTRUCT ASPHALT PAVEMENT AND CONCRETE DUMPSTER PAD.
5. TURF AND DRAINAGE GRADING.
6. TOPSOIL AND SODDING.

**PHASE 1C AIRCRAFT MOVEMENTS:**

1. THE FBO/TERMINAL APRON SHALL REMAIN OPERATIONAL DURING THIS PHASE.
2. THE EXISTING T-HANGARS SHALL REMAIN FULLY ACCESSIBLE DURING THIS PHASE.
3. ACCESS THROUGH GATE 1 SHALL BE LIMITED TO CONSTRUCTION VEHICLES ONLY.

**PHASE 1C LIMITS OF WORK:**

1. PHASE WORK LIMITS ARE GENERALLY AS SHOWN.

**LEGEND**

- AIRCRAFT MOVEMENT
- TYPE 1 LOW- PROFILE BARRICADE (FOR THE STATED WORK AREA)
- HAUL ROUTE
- CONSTRUCTION TRAFFIC SIGN
- SECURITY GUARD

REVISIONS:			EOR
REV. NO	DATE	DESCRIPTION:	
	12/12/2022	CONFORMED DOCUMENTS	JCM
	10/12/2023	RECORD DRAWINGS	JCM

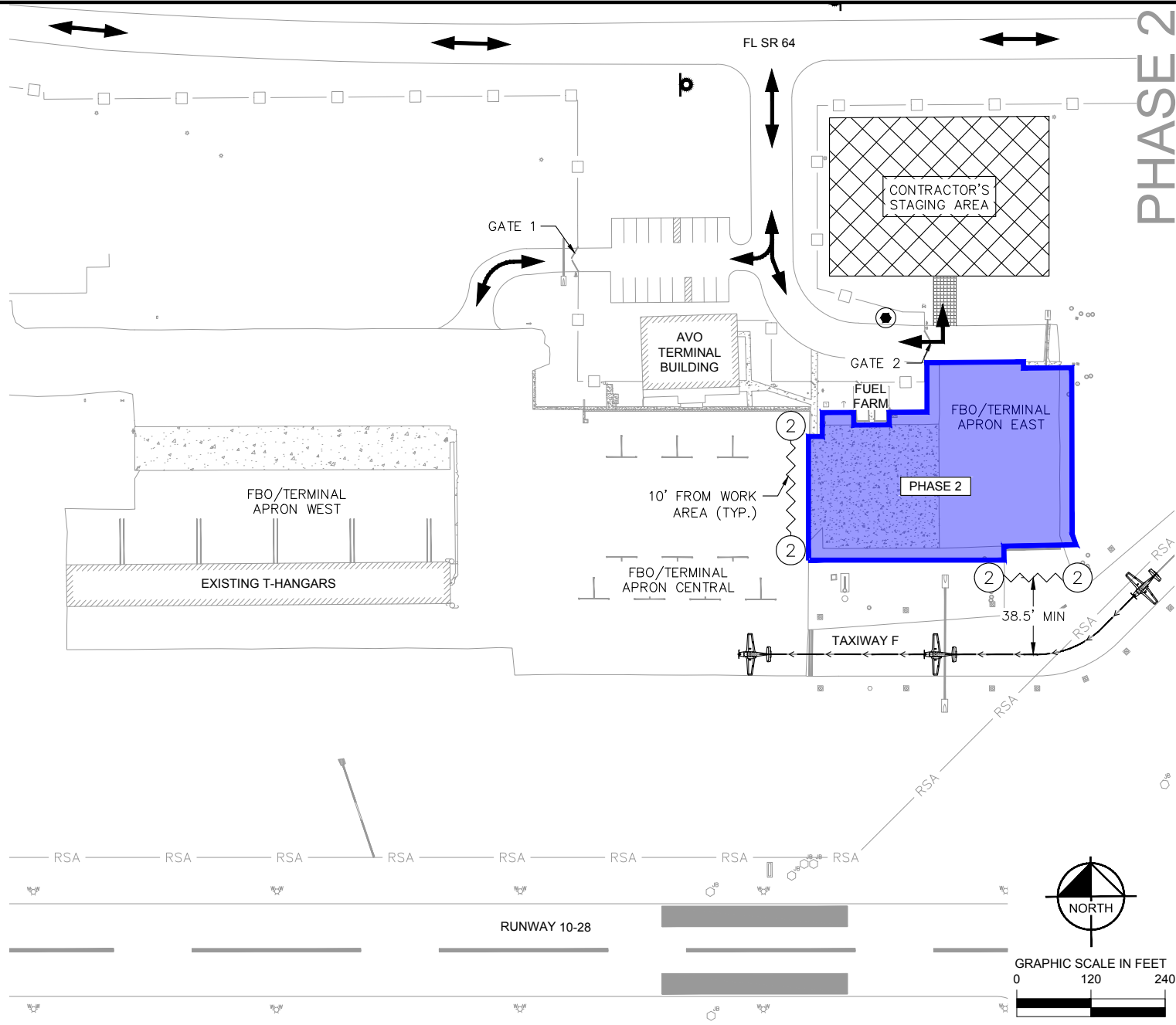


FBO/TERMINAL APRON  
REHABILITATION  
CITY OF AVON PARK  
AVON PARK, FL

**Kimley»Horn**  
©2021 KIMLEY-HORN AND ASSOCIATES, INC.  
655 NORTH FRANKLIN ST., SUITE 150  
TAMPA, FLORIDA 33602  
PHONE: 813-620-1460  
WWW.KIMLEY-HORN.COM REGISTRY NO. 35106

JOB NO:	046464008	PHASING PLAN - PHASE 1A, 1B & 1C (BASE BID)
DRAWN:	BJG	
DESIGN:	BJG	P112
CHECKED:	JCM	
DATE:	3/10/2022	

K:\TAM\_Aviation\1\_Engn\AVO\2019 - On Call Engineering\Task Orders\FY 2021 - Apron Rehab\CAD\Plan Sheets\PHASING 2 & 3.dwg Tab: P113 Oct 12, 2023 - 10:04am Plotted by: katie.dodge



LEGEND

- AIRCRAFT MOVEMENT
- TYPE 1 LOW- PROFILE BARRICADE (FOR THE STATED WORK AREA)
- HAUL ROUTE
- CONSTRUCTION TRAFFIC SIGN
- SECURITY GUARD

PHASE 2 DESCRIPTION:

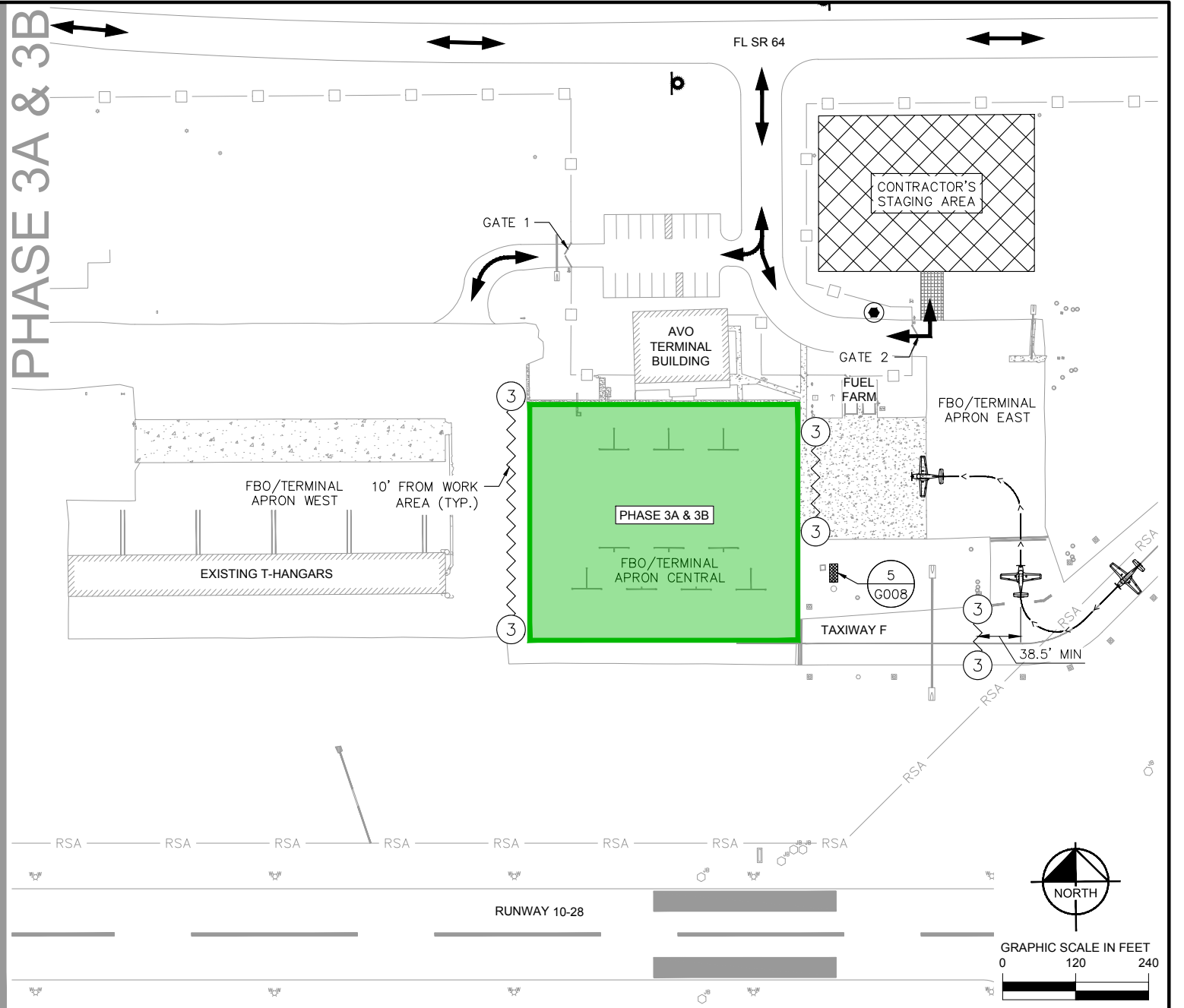
1. PHASE 2 TO BEGIN IMMEDIATELY FOLLOWING PHASE 1C.
2. SET UP MOT, EROSION CONTROL INSTALLATION, AND CLEARING AND GRUBBING.
3. FULL DEPTH ASPHALT PAVEMENT REMOVAL OF APRON PAVEMENT AND AND FULL DEPTH REMOVAL OF CONCRETE SLAB WITHIN THE SPECIFIED LIMITS.
4. SUBGRADE PREPARATION AND INSTALLATION OF BASE MATERIAL FOR NEW ASPHALT PAVEMENT SECTION.
5. APPLY ASPHALT SURFACE COURSE.
6. TOPSOIL AND SODDING.

PHASE 2 AIRCRAFT MOVEMENTS:

1. THE EAST PORTION OF THE FBO/TERMINAL APRON WILL BE CLOSED DURING THIS PHASE.
2. ACCESS THROUGH GATE 2 SHALL BE LIMITED TO CONSTRUCTION VEHICLES ONLY.

PHASE 2 LIMITS OF WORK:

1. PHASE WORK LIMITS ARE GENERALLY AS SHOWN.



PHASE 3A & 3B DESCRIPTION:

1. PHASE 3 IS INCLUSIVE OF ALL CONSTRUCTION ITEMS ENCOMPASSING PHASE 3A & 3B
2. PHASE 3A TO BEGIN IMMEDIATELY FOLLOWING PHASE 2.
3. PHASE 3B TO BEGIN IMMEDIATELY FOLLOWING 30 DAY MATERIAL CURE TIME.
4. SET UP MOT CONTROLS.
5. REMOVE EXISTING PAVEMENT MARKINGS
6. APPLY ASPHALT REJUVENATOR TO DESIGNATED AREAS.
7. INSTALL FINAL PAVEMENT MARKINGS AS PHASE 3B FOLLOWING 30-DAY CURE OF ASPHALT REJUVENATOR.

PHASE 3A & 3B AIRCRAFT MOVEMENTS:

1. THE CENTRAL PORTION OF THE FBO/TERMINAL APRON WILL BE CLOSED DURING THIS PHASE.

PHASE 3A & 3B LIMITS OF WORK:

1. PHASE WORK LIMITS ARE GENERALLY AS SHOWN.

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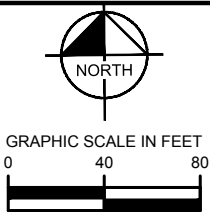
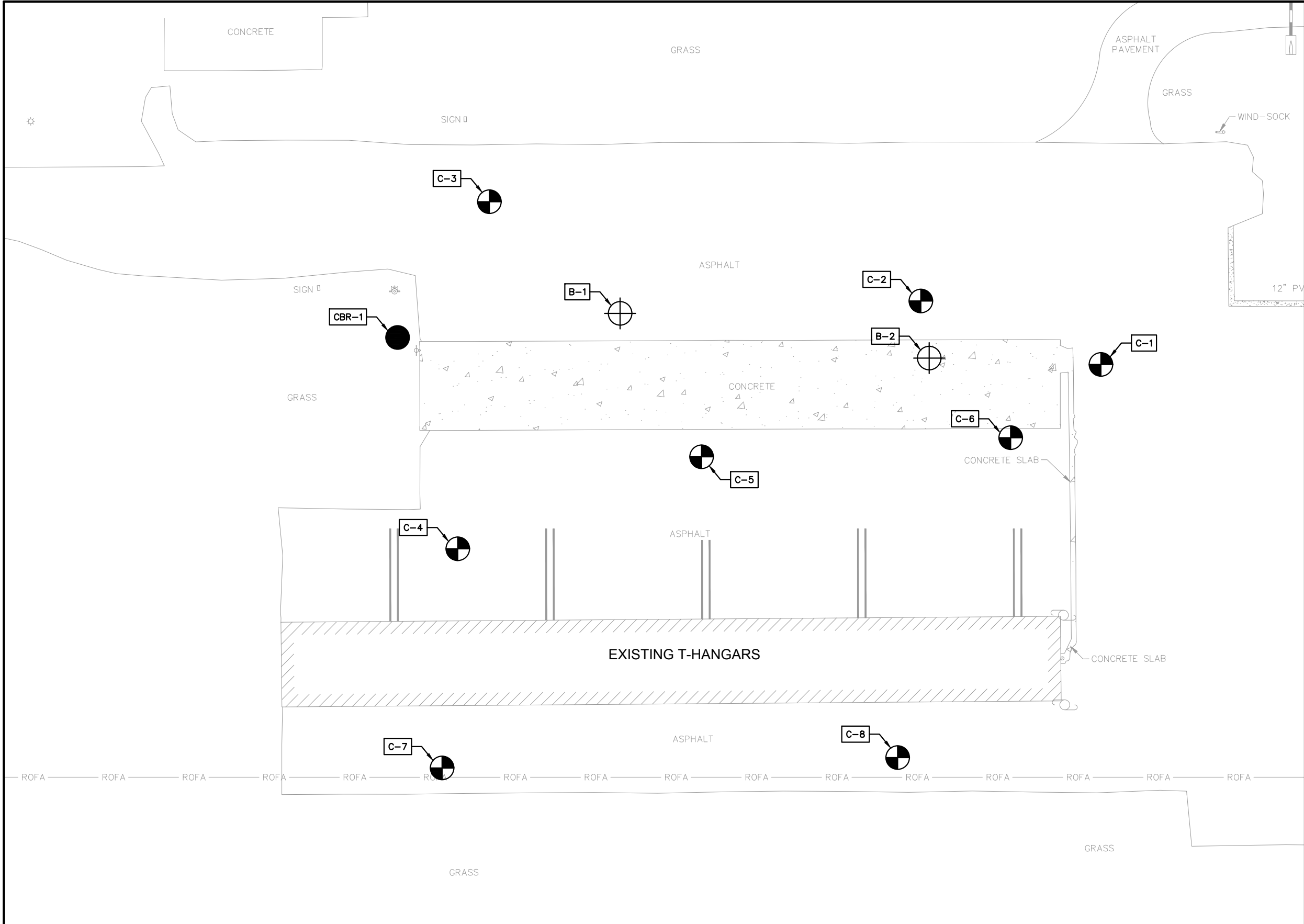
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PHASING PLAN - PHASE 2 & 3  
(BID ADD ALTERNATE 1)

P113

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NOTES

1. ALL CORING, BORING, AND CBR LOCATIONS AND DATA CAN BE FOUND IN THE "REPORT OF GEOTECHNICAL ENGINEERING EVALUATION" BY GEOTECHNICAL AND ENVIRONMENTAL CONSULTANTS, INC. (GEC) DATED DECEMBER 8, 2021.

LEGEND

- CBR LOCATION  
⊕ BORING LOCATION  
◐ CORE LOCATION

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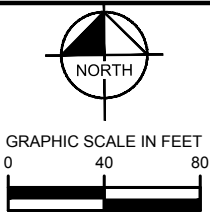
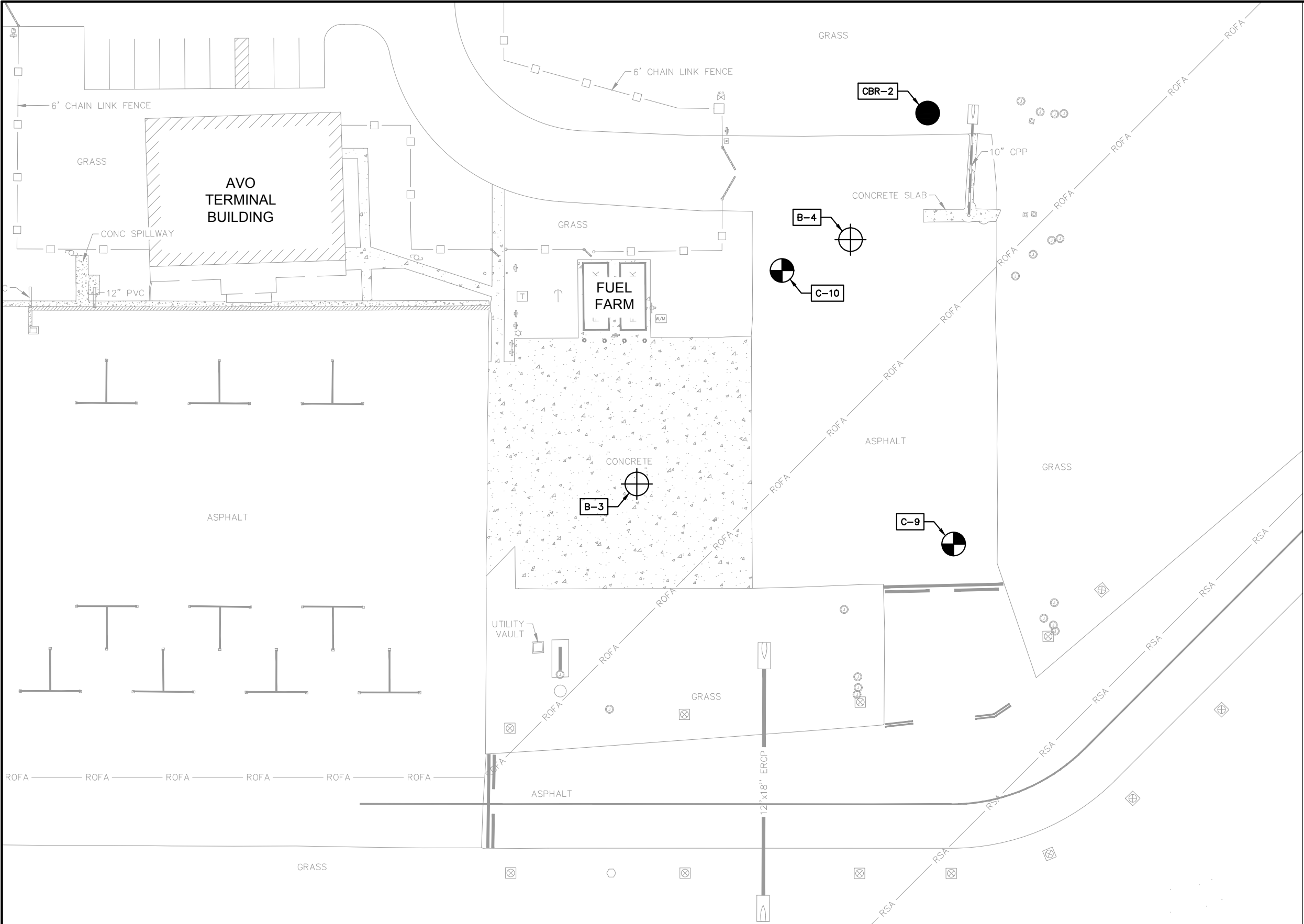
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GEOTECHNICAL CORING AND  
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B110

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- ⊕ BORING LOCATION
- ⊗ CORE LOCATION

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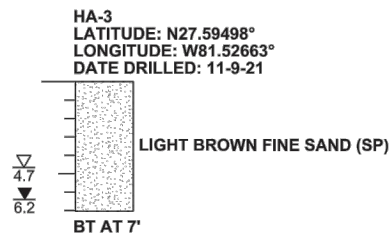
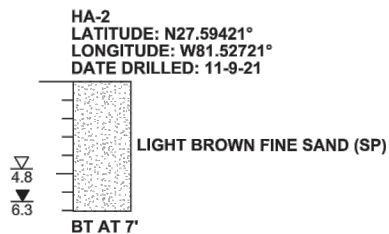
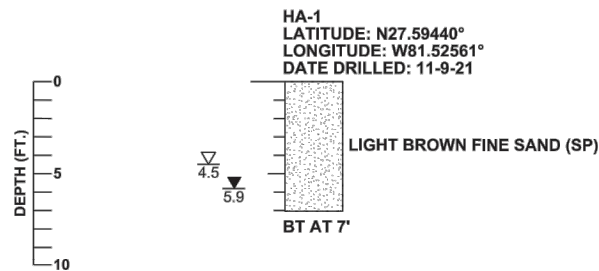
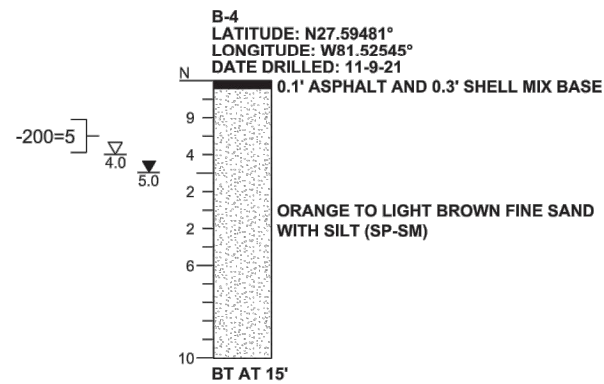
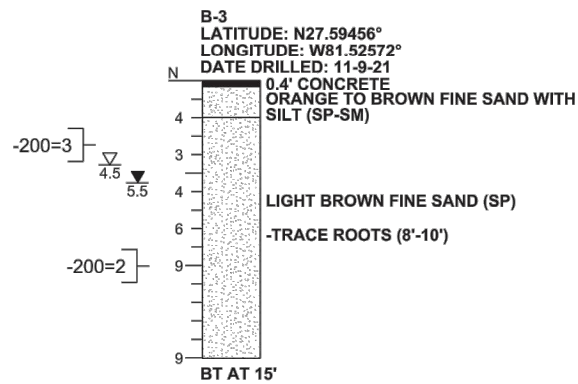
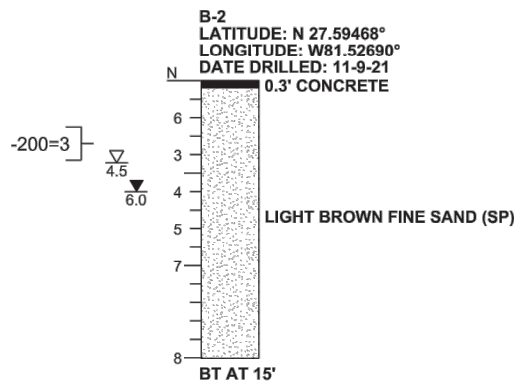
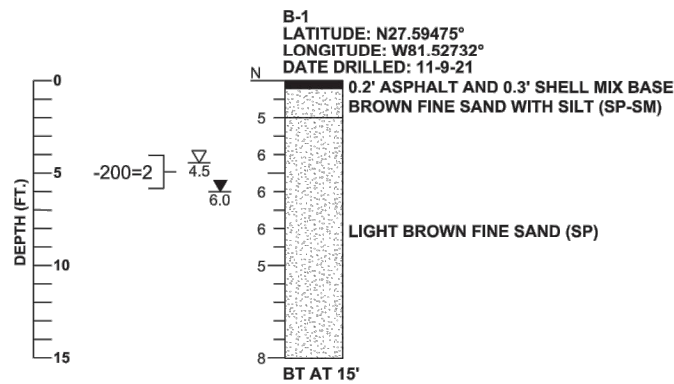
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GEOTECHNICAL CORING AND  
BORING PLAN

**B111**



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#### LEGEND

- N STANDARD PENETRATION RESISTANCE, BLOWS PER FOOT
- ▽ 4.5 ESTIMATED SEASONAL HIGH GROUNDWATER DEPTH (FT.)
- ▼ 5.9 ENCOUNTERED GROUNDWATER DEPTH (FT.) 24 HRS. AFTER DATE DRILLED
- BT BORING TERMINATED AT DEPTH INDICATED
- 200= PERCENT PASSING NO. 200 U.S. STANDARD SIEVE
- SAND

#### GENERAL NOTES

SUBSURFACE CONDITIONS SHOWN ON THE BORINGS REPRESENT THE CONDITIONS ENCOUNTERED AT THE BORING LOCATIONS. ACTUAL CONDITIONS BETWEEN THE BORINGS MAY VARY FROM THOSE SHOWN. UNIFIED SOIL CLASSIFICATIONS SHOWN ON THE BORINGS ARE BASED ON VISUAL EXAMINATION AND THE LABORATORY TESTING SHOWN.

STANDARD PENETRATION TEST BORINGS WERE PERFORMED IN ACCORDANCE WITH ASTM D-1586. STANDARD PENETRATION RESISTANCES ARE SHOWN ON THE BORINGS AT THE TEST DEPTHS IN IN BLOWS PER FOOT UNLESS OTHERWISE NOTED.

STABILIZED GROUND WATER DEPTHS NOT OBTAINED AT LOCATIONS B-1 TO B-4 DUE TO BOREHOLES BEING BACKFILLED UPON COMPLETION.

BORING LOCATIONS WERE NOT SURVEYED. BORING LOCATIONS WERE ESTABLISHED IN THE FIELD USING A SUB-METER ACCURACY GPS UNIT (TRIMBLE GEO XH).

SPLIT SPOON SAMPLER:  
INSIDE DIAMETER: 1.375 IN.  
OUTSIDE DIAMETER: 2.0 IN.  
AVERAGE HAMMER DROP: 30 IN.  
HAMMER WEIGHT: 140 LBS.

#### CORRELATION OF STANDARD PENETRATION RESISTANCE WITH RELATIVE DENSITY AND CONSISTENCY OF SOIL

AUTOMATIC HAMMER		
GRANULAR SOILS	N VALUE (blows per foot)	RELATIVE DENSITY
SANDS	0-3	VERY LOOSE
	3-8	LOOSE
	8-24	MEDIUM DENSE
	24-40	DENSE
	OVER 40	VERY DENSE
AUTOMATIC HAMMER		
NON-GRANULAR SOILS	N VALUE (blows per foot)	CONSISTENCY
SILTS, CLAYS, MUCK, PEAT	0-1	VERY SOFT
	1-3	SOFT
	3-6	FIRM
	6-12	STIFF
	12-24	VERY STIFF
	OVER 24	HARD

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FBO/TERMINAL APRON  
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GEOTECHNICAL CORING AND  
BORING LOG DATA

B112

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State of Florida Department of Transportation																			
PAVEMENT EVALUATION AND CONDITION DATA SHEET																			
Project No.: 4894G					Cored By: Geotechnical and Environmental Consultants, Inc.					Date: 11/9/2021					Page No.: 1 of 1				
Project: Avon Park Executive Airport					Road No.					From:					To:				
County: Highlands					Begin MP:					End M.P.:					Length:				
Core No.	Approximate Latitude	Approximate Longitude	Pavement Layer (in.)							Base		Crack				Pavt Cond.	Rut Depth (in)	Cross Slope (%)	Comments
									Core Length (in)	Type	Thick-ness (in)	Depth (in)	Type	Class	Extent				
C-1	N27.59468	W81.52674							1.25	SM	7.25	FD	S/BL/AL	II	M	P			Subgrade = SP
C-2	N27.59475	W81.52696							2.5	SM	4.0	0.5	S/BR	II	S	P			Subgrade = SP
C-3	N27.59486	W81.52749							1.75	SC	10.5	FD	S/BR	III	S	P			Subgrade = SP
C-4	N27.59448	W81.52753							1.13	SM	6.125	FD	S	IB	M	P			Subgrade = SP
C-5	N27.59458	W81.52723							1.5	LR	3.5	---	S/BL	II	M	P			Subgrade = SP
C-6	N27.59460	W81.52685							2.5	LR	3.0	---	AL	II	S	P			Subgrade = SP
C-7	N27.59424	W81.52755							1.0	SM	8.0	FD	S/BR	III	S	P			Subgrade = SP
C-8	N27.59425	W81.52699							1.0	SM	6.5	FD	S/BR	II	M	P			Subgrade = SP
C-9	N27.59448	W81.52532							1.0	LR	3.5	FD	S	II	M	P			Subgrade = SP
C-10	N27.59478	W81.52553							0.75	LR	5.75	FD	S/BR	III	S	P			Subgrade = SM
<b>Remarks:</b>																			
<u>Base Types:</u> LR = Limerock; SC = Soil Cement; SM = Shell Mix, ABC = Asphalt Base; SAHM = Sand Asphalt Hot Mix; SBRM = Sand Bituminous Road Mix; CC = Crushed Concrete; NB = No Base																			
<u>Crack Depth:</u> FD = Full depth crack to the base																			
<u>Crack Types:</u> A = Alligator; Bl = Block; Br = Branch; SL = Single Longitudinal; ST = Single Transverse; R = Reflective; J = Joint; OGFC = Open-Graded FC Stress Crack																			
<u>Crack Class:</u> IB = ≤ 1/8"; II = > 1/8" to ≤ 1/4"; III = > 1/4"																			
<u>Crack Extent:</u> L = Light; M = Moderate; S = Severe																			
<u>Pavement Condition:</u> G = Good; F = Fair; P = Poor																			

NOT FOR CONSTRUCTION

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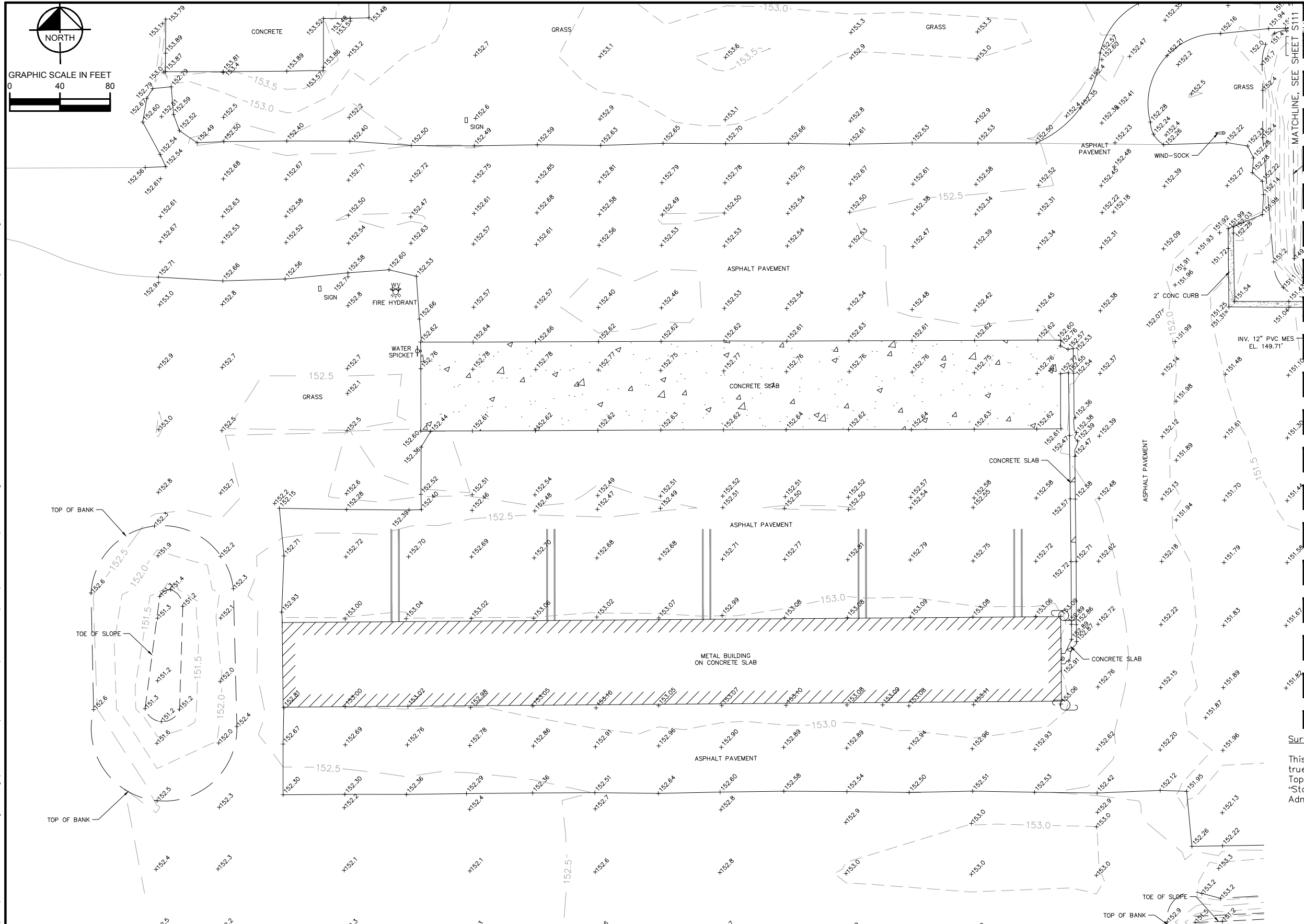
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GEOTECHNICAL CORING AND BORING LOG DATA

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**GENERAL NOTES:**

1. FIELD WORK FOR THIS SURVEY COMPLETED NOVEMBER, 2021.
2. ORIGINAL SHEET SIZE IS 22"X34".
3. RIGHT-OF-WAY LINES INDICATED HEREIN ARE BASED ON AVAILABLE PUBLIC RECORD INFORMATION TOGETHER WITH FOUND BOUNDARY MONUMENTATION.
4. THIS SURVEY DRAWING WAS PREPARED FOR THE EXCLUSIVE USE OF THE PARTY OR PARTIES CERTIFIED TO BELOW FOR THE EXPRESS PURPOSE STATED HEREON AND/OR CONTAINED IN THE CONTRACT BETWEEN BARNES, FERLAND, AND ASSOCIATES, INC. AND THE CLIENT FOR THIS PROJECT. COPYING, DISTRIBUTING AND/OR USING THIS DRAWING, IN WHOLE OR IN PART FOR ANY PURPOSE OTHER THAN ORIGINALLY INTENDED WITHOUT WRITTEN CONSENT FROM BARNES, FERLAND AND ASSOCIATES, INC. IS STRICTLY PROHIBITED AND RENDERS THE SURVEYOR'S CERTIFICATION, SIGNATURE AND SEAL NULL AND VOID. ANY QUESTIONS CONCERNING THE CONTENT OR PURPOSE OF THIS DRAWING SHOULD BE DIRECTED TO BARNES, FERLAND AND ASSOCIATES, INC.

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2. LANDS WERE NOT RESEARCHED BY THIS FIRM FOR MATTERS SUCH AS OWNERSHIP, EASEMENTS, RIGHT OF WAY OR OTHER MATTERS IN THE PUBLIC RECORDS THAT MAY AFFECT THESE LANDS. A TITLE REPORT WAS NOT PROVIDED TO THE SURVEYOR AT THE TIME OF SURVEY.
3. COORDINATES ARE RELATIVE TO THE FLORIDA STATE PLANE COORDINATE SYSTEM, EAST ZONE, NORTH AMERICAN DATUM OF 1983/1990 ADJUSTMENT.
4. ELEVATIONS SHOWN HEREON ARE RELATIVE TO NORTH AMERICAN VERTICAL DATUM OF 1988 (NAVD 88) WITH DIRECT TIES TO THE FOLLOWING PUBLISHED NATIONAL GEODETIC SURVEY BENCHMARKS:  
  
DESIGNATION: N 115 - PID: AF6467  
ELEVATION: 152.83'  
  
DESIGNATION: AVONPORT - PID: AF7411  
ELEVATION: 152.16'
5. LAST DAY IN THE FIELD: 11/4/2021.
6. GRAPHIC SYMBOLS SHOWN HEREON MAY NOT BE TO SCALE.
7. THIS TOPOGRAPHIC SURVEY IS CERTIFIED TRUE AND CORRECT TO: KIMLEY-HORN AND ASSOCIATES, INC.
8. NO UNDERGROUND STRUCTURES, UTILITIES OR FOUNDATIONS WERE LOCATED BY THIS SURVEY.
9. THIS TOPOGRAPHIC SURVEY IS OF THE AREA AS SPECIFIED BY THE CLIENT ONLY.
10. THIS IS NOT A BOUNDARY SURVEY AND DOES NOT PURPORT TO BE ONE.

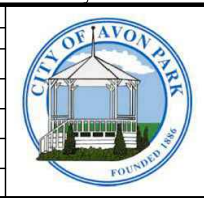
Surveyor Certification

This Topographic Survey was prepared under my direction and is true and correct to the best of my knowledge and belief. This Topographic Survey was performed in conformance with the "Standards of Practice" as contained in Chapter 5J-17, Florida Administrative Code, pursuant to Florida Statute 472.

genel j sturgeon Date: 2022.01.06  
12:15:17-05'00'

Genel J. Sturgeon, PSM Date  
Florida Surveyor and Mapper PSM 5866  
Barnes Ferland and Associates LB 7774

REVISIONS:			EOR
REV. NO	DATE	DESCRIPTION:	
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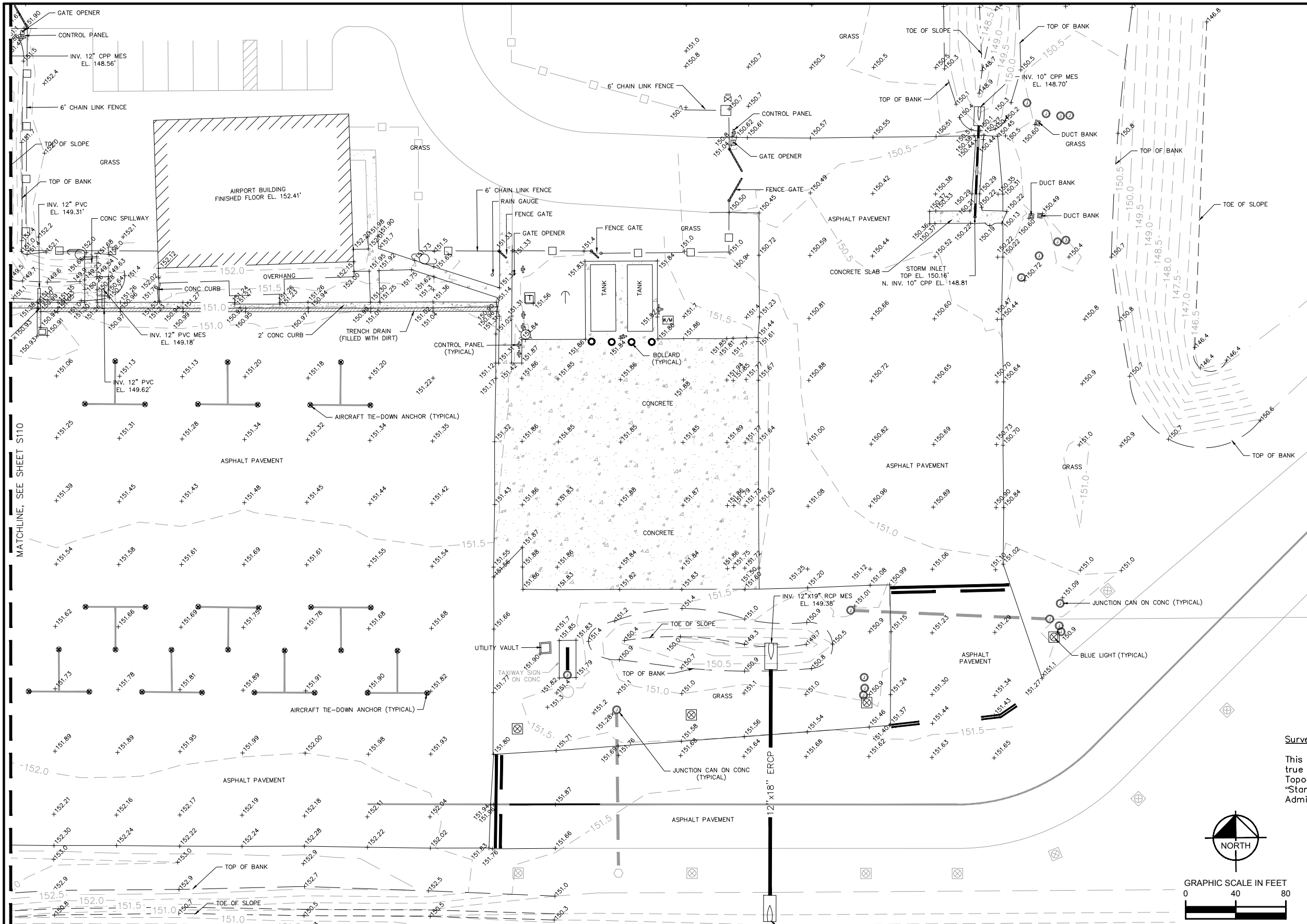
**BFA** Environmental Consultants  
Barnes, Ferland and Associates, Inc.  
1230 Hillcrest St., Orlando, Florida 32803  
PH: (407) 596-8028 FAX: (407) 895-1222  
CERTIFICATE OF AUTHORIZATION NUMBER: LB 7774

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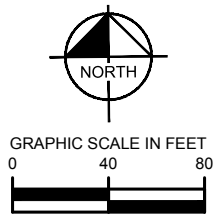
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Florida Surveyor and Mapper PSM 5866  
Barnes Ferland and Associates LB 7774



REVISIONS:		
REV. NO	DATE	DESCRIPTION:
		EOR
		JCM
		JCM



FBO/TERMINAL APRON  
REHABILITATION  
CITY OF AVON PARK  
AVON PARK, FL

**BFA** Environmental Consultants  
Barnes, Ferland and Associates, Inc.  
1230 Hillcrest St., Orlando, Florida 32803  
PH: (407) 956-8028 FAX: (407) 956-1822  
CERTIFICATE OF AUTHORIZATION NUMBER: LB 7774

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655 NORTH FRANKLIN ST., SUITE 150  
TAMPA, FLORIDA 33602  
PHONE: 813-620-1460  
WWW.KIMLEY-HORN.COM REGISTRY NO. 35106

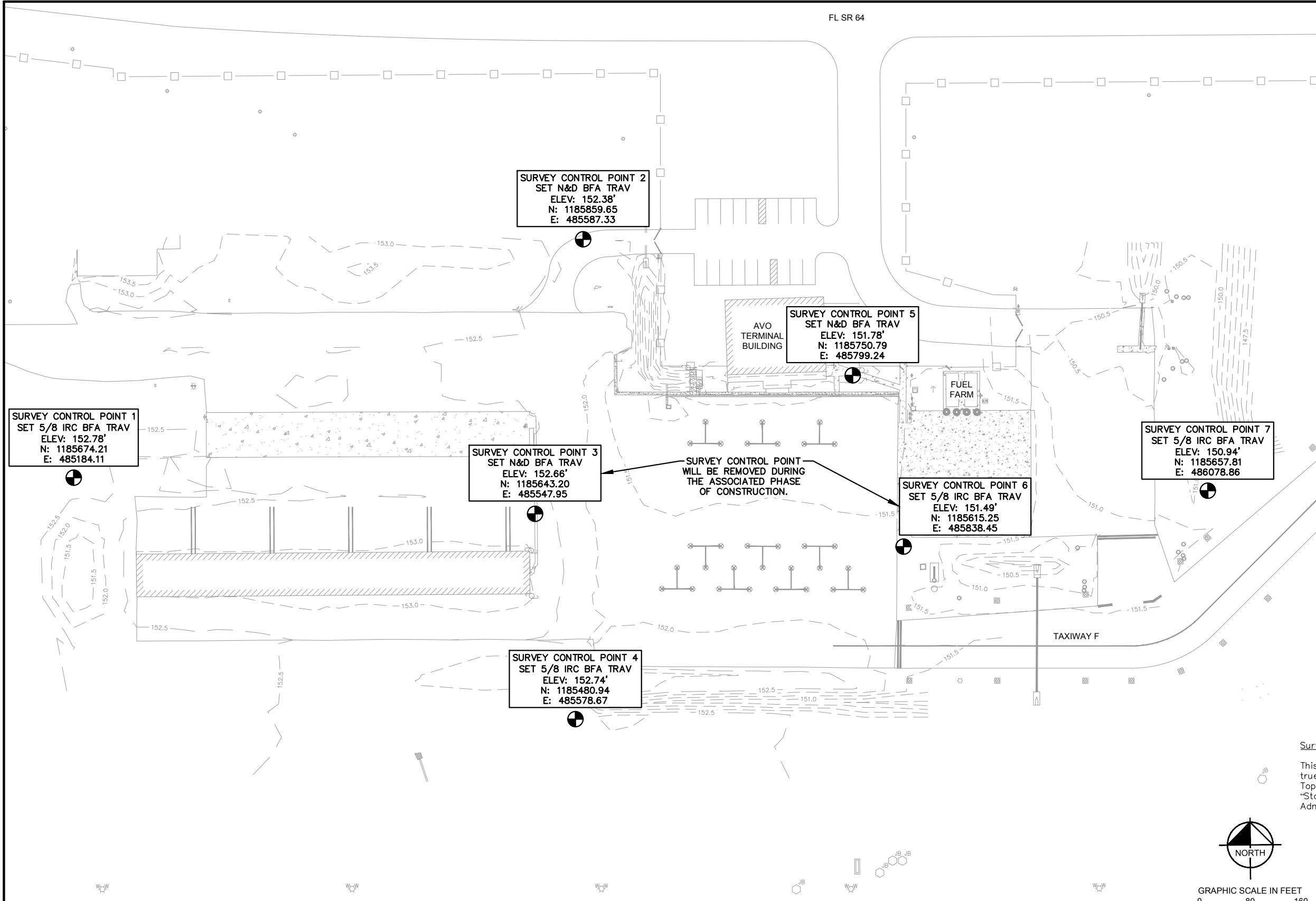
JOB NO:	046464008
DRAWN:	BJG
DESIGN:	BJG
CHECKED:	JCM
DATE:	3/10/2022

EXISTING CONDITIONS SURVEY

S111



K:\TAM\_Aviation\1\_Engr\_AVO\2019 - On Call Engineering\Task Orders\FY 2021 - Apron Rehab\CAD\Plan Sheets\HORIZONTAL AND VERTICAL CONTROL.dwg Tab: S112 Oct 12, 2023 - 10:04am Plotted by: katie.dodge



#### GENERAL NOTES:

1. FIELD WORK FOR THIS SURVEY COMPLETED NOVEMBER, 2021.
2. ORIGINAL SHEET SIZE IS 22"X34".
3. RIGHT-OF-WAY LINES INDICATED HEREIN ARE BASED ON AVAILABLE PUBLIC RECORD INFORMATION TOGETHER WITH FOUND BOUNDARY MONUMENTATION.
4. THIS SURVEY DRAWING WAS PREPARED FOR THE EXCLUSIVE USE OF THE PARTY OR PARTIES CERTIFIED TO BELOW FOR THE EXPRESS PURPOSE STATED HEREON AND/OR CONTAINED IN THE CONTRACT BETWEEN BARNES, FERLAND, AND ASSOCIATES, INC. AND THE CLIENT FOR THIS PROJECT. COPYING, DISTRIBUTING AND/OR USING THIS DRAWING, IN WHOLE OR IN PART FOR ANY PURPOSE OTHER THAN ORIGINALLY INTENDED WITHOUT WRITTEN CONSENT FROM BARNES, FERLAND AND ASSOCIATES, INC. IS STRICTLY PROHIBITED AND RENDERS THE SURVEYOR'S CERTIFICATION, SIGNATURE AND SEAL NULL AND VOID. ANY QUESTIONS CONCERNING THE CONTENT OR PURPOSE OF THIS DRAWING SHOULD BE DIRECTED TO BARNES, FERLAND AND ASSOCIATES, INC.

#### SURVEY NOTES:

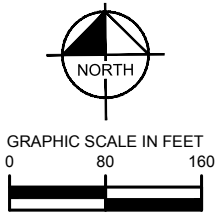
1. THIS TOPOGRAPHIC SURVEY IS NOT VALID UNLESS SIGNED AND EMBOSSED WITH THE RAISED SEAL OF A FLORIDA LICENSED SURVEYOR AND MAPPER.
2. LANDS WERE NOT RESEARCHED BY THIS FIRM FOR MATTERS SUCH AS OWNERSHIP, EASEMENTS, RIGHT OF WAY OR OTHER MATTERS IN THE PUBLIC RECORDS THAT MAY AFFECT THESE LANDS. A TITLE REPORT WAS NOT PROVIDED TO THE SURVEYOR AT THE TIME OF SURVEY.
3. COORDINATES ARE RELATIVE TO THE FLORIDA STATE PLANE COORDINATE SYSTEM, EAST ZONE, NORTH AMERICAN DATUM OF 1983/1990 ADJUSTMENT.  
  
DESIGNATION: N 115 - PID: AF6467  
ELEVATION: 152.83'  
  
DESIGNATION: AVONPORT - PID: AF7411  
ELEVATION: 152.16'
4. ELEVATIONS SHOWN HEREON ARE RELATIVE TO NORTH AMERICAN VERTICAL DATUM OF 1988 (NAVD 88) WITH DIRECT TIES TO THE FOLLOWING PUBLISHED NATIONAL GEODETIC SURVEY BENCHMARKS:  
  
DESIGNATION: N 115 - PID: AF6467  
ELEVATION: 152.83'  
  
DESIGNATION: AVONPORT - PID: AF7411  
ELEVATION: 152.16'
5. LAST DAY IN THE FIELD: 11/4/2021.
6. GRAPHIC SYMBOLS SHOWN HEREON MAY NOT BE TO SCALE.
7. THIS TOPOGRAPHIC SURVEY IS CERTIFIED TRUE AND CORRECT TO: KIMLEY-HORN.
8. NO UNDERGROUND STRUCTURES, UTILITIES OR FOUNDATIONS WERE LOCATED BY THIS SURVEY.
9. THIS TOPOGRAPHIC SURVEY IS OF THE AREA AS SPECIFIED BY THE CLIENT ONLY.
10. THIS IS NOT A BOUNDARY SURVEY AND DOES NOT PURPORT TO BE ONE.

#### Surveyor Certification

This Topographic Survey was prepared under my direction and is true and correct to the best of my knowledge and belief. This Topographic Survey was performed in conformance with the "Standards of Practice" as contained in Chapter 5J-17, Florida Administrative Code, pursuant to Florida Statute 472.

genel j sturgeon Date: 2022.01.06  
12:15:17-05'00'

Genel J. Sturgeon, PSM Date  
Florida Surveyor and Mapper PSM 5866  
Barnes Ferland and Associates LB 7774



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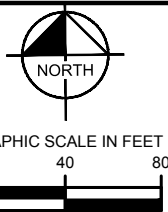
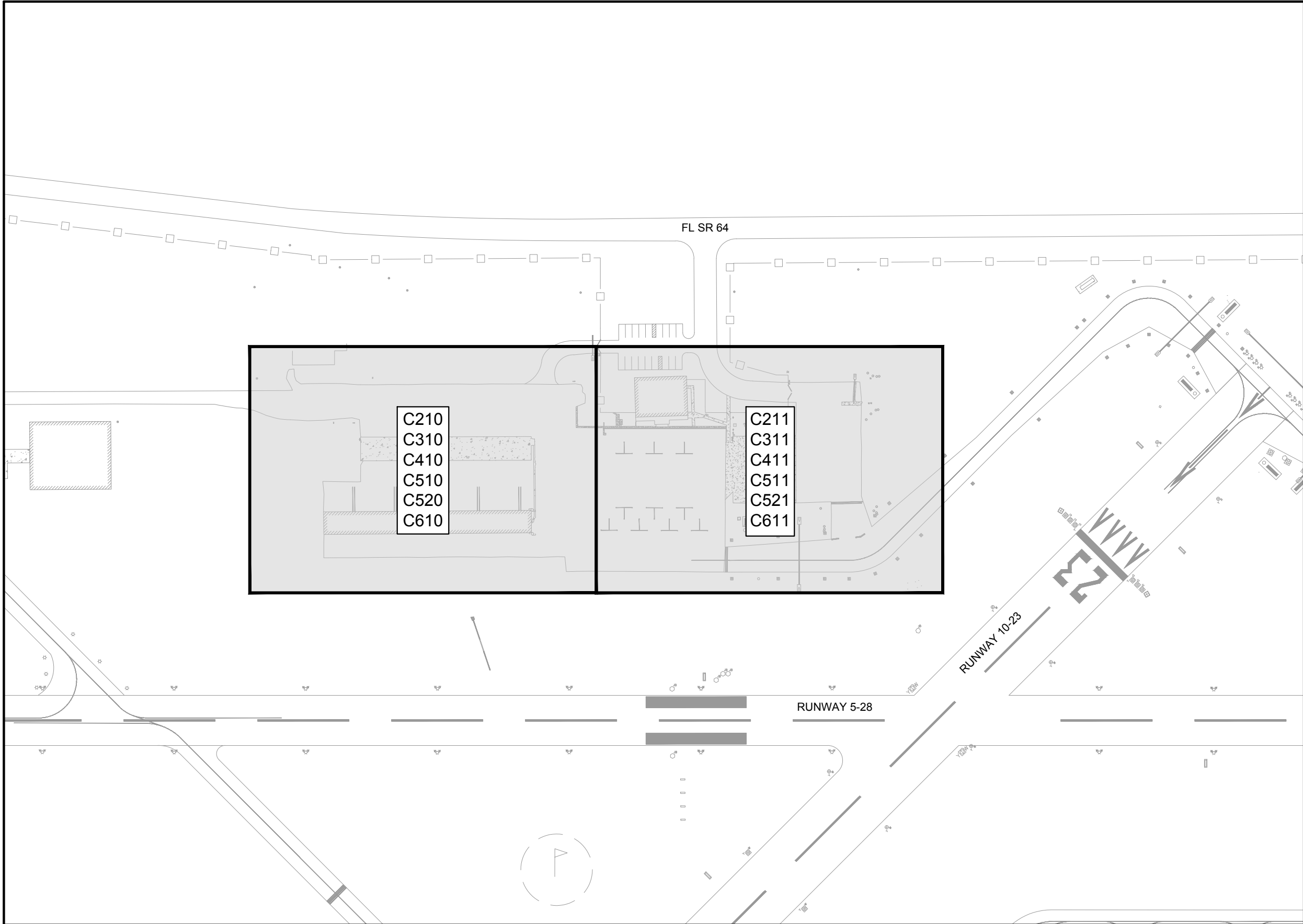
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HORIZONTAL AND VERTICAL  
CONTROL PLAN

S112

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**CIVIL DRAWINGS**

- 200 SERIES = EROSION CONTROL PLANS
- 300 SERIES = DEMOLITION PLANS
- 400 SERIES = GEOMETRY AND PAVING PLANS
- 500 SERIES = GRADING AND DRAINAGE PLANS
- 520 SERIES = PAVEMENT ELEVATION PLANS
- 600 SERIES = PAVEMENT MARKING PLANS
- E SERIES = ELECTRICAL PLANS

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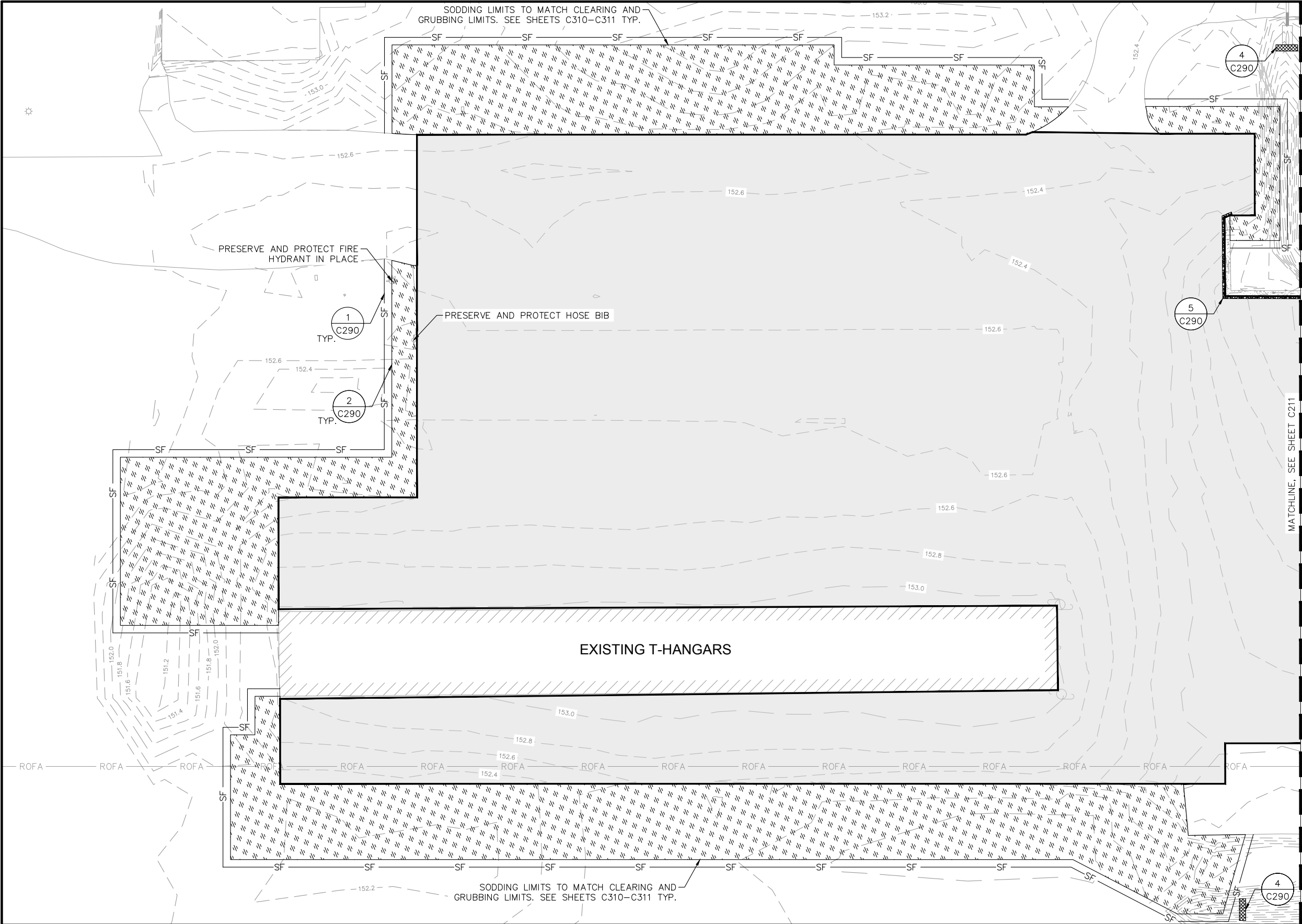
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PROJECT SHEET MAP

C100

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NOTES

1. ALL EROSION AND SEDIMENTATION CONTROL DEVICES SHALL BE IN PLACE PRIOR TO THE START OF ANY CONSTRUCTION ACTIVITIES AND SHALL REMAIN IN PLACE UNTIL ALL CONSTRUCTION ACTIVITIES HAVE FINISHED.
2. CONTRACTOR SHALL INSTALL EROSION AND SEDIMENTATION CONTROL SYSTEMS WITHIN THE LIMITS OF EACH PHASE OF WORK IN COORDINATION WITH THE RPR AND OWNER.
3. CONTRACTOR SHALL MAINTAIN EROSION AND SEDIMENTATION CONTROL DEVICES THROUGHOUT THE DURATION OF THE PROJECT AND PROVIDE ADDITIONAL BMP'S AS REQUIRED TO THE SATISFACTION OF AND AT NO ADDITIONAL COST TO THE OWNER.
4. SODDING SHALL BE APPLIED TO ALL AREAS THAT HAVE BEEN IMPACTED BY CONSTRUCTION ACTIVITIES. SODDING AND TOPSOIL SHALL BE APPLIED AT A MINIMUM OF 10' FROM ALL CONSTRUCTION ACTIVITIES THAT ARE ADJACENT TO GRASS AREAS.
5. SILT FENCE IS SHOWN OFFSET ON THE PLANS FOR CLARITY. ALL SILT FENCE SHALL BE PLACED AT THE LIMITS OF ALL CONSTRUCTION ACTIVITIES AS INDICATED ON THE PLANS.

LEGEND

- PAVEMENT WORK LIMITS
- APPROXIMATE SODDING LIMITS, ITEM T-904-1
- INLET PROTECTION, SEE DETAIL 3, SHEET C290
- HAY BALES, SEE DETAIL 1, SHEET C291
- SF SILT FENCE, SEE DETAIL 1, SHEET C290
- SOCK DRAIN INLET PROTECTION, SEE DETAIL 5, SHEET C290

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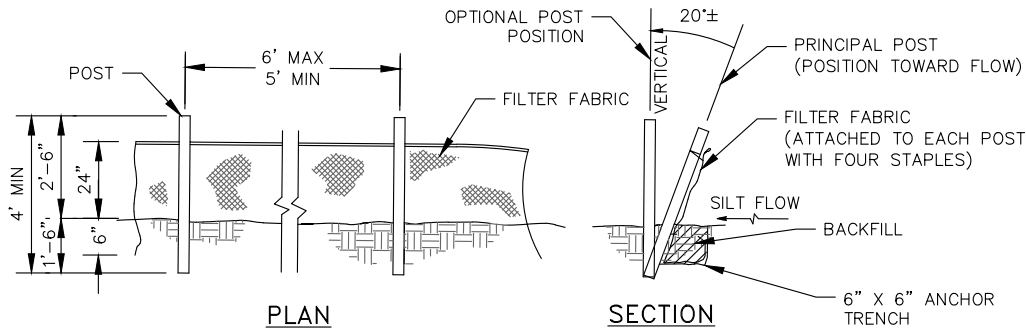
EROSION CONTROL PLANS

C210





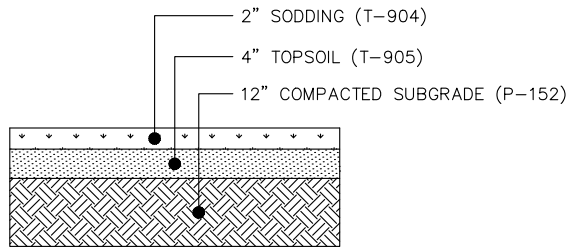
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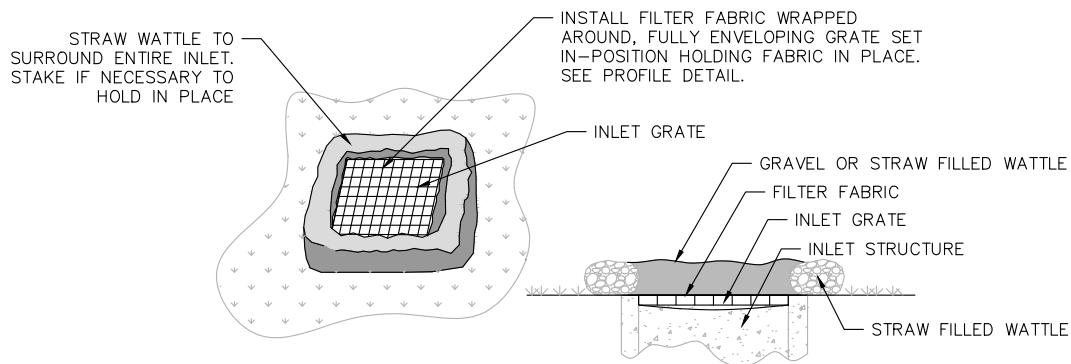
NOTES:

FOR EACH END POST, FABRIC SHALL BE FOLDED AROUND 2 POSTS ONE FULL TURN AND SECURED WITH FOUR STAPLES.

1 SILT FENCE  
C290 NOT TO SCALE



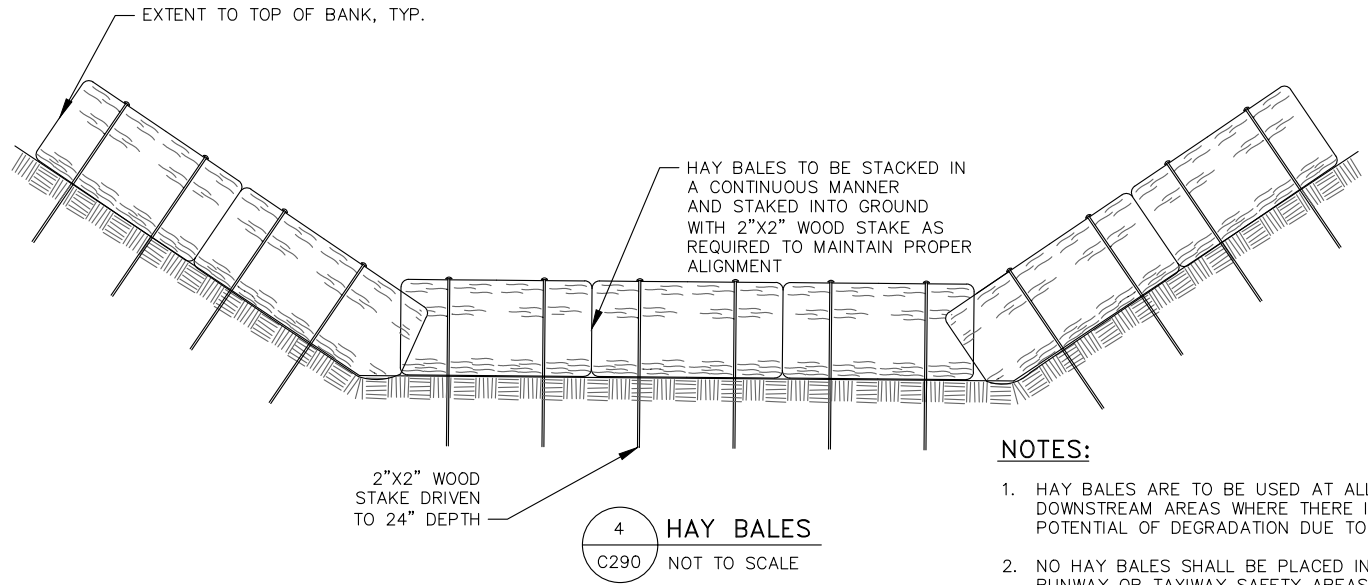
2 SODDING AND MAT SECTION  
C290 NOT TO SCALE



NOTES:

FILTER FABRIC 6" STRAW "SNAKE" WATTLE FOR AREAS WHERE INLET IS IN PERVIOUS AREAS LOCATED WITHIN THE RSA OR TOFA. "DANDY" SACK FOR AREAS LOCATED IN IMPERVIOUS PAVEMENT AREAS SUBJECT TO AIRCRAFT OR VEHICULAR TRAFFIC.

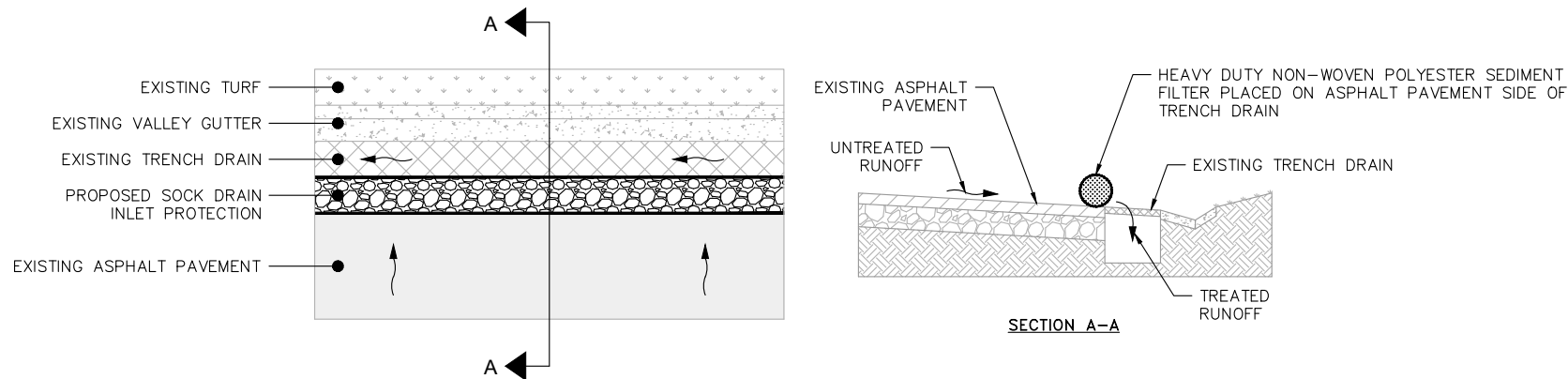
3 INLET GRATE FILTER FABRIC AND WADDLE SEDIMENT CONTROL DEVICE  
C290 NOT TO SCALE



NOTES:

- HAY BALES ARE TO BE USED AT ALL DOWNSTREAM AREAS WHERE THERE IS A POTENTIAL OF DEGRADATION DUE TO EROSION.
- NO HAY BALES SHALL BE PLACED IN ACTIVE RUNWAY OR TAXIWAY SAFETY AREAS.

4 HAY BALES  
C290 NOT TO SCALE



NOTES:

- SOCK DRAIN INLET SHALL BE INSPECTED AFTER ALL RAIN EVENTS AND SHALL BE MAINTAINED TO BE FULLY OPERATIONAL AT ALL TIMES.
- MATERIAL INSIDE OF THE SOCK DRAIN SHALL BE 2"-3" CLEAN STONE GRAVEL OR MULCHED WOOD CHIPS NO LARGER THAN 2"-3".

5 SOCK DRAIN INLET PROTECTION  
C290 NOT TO SCALE

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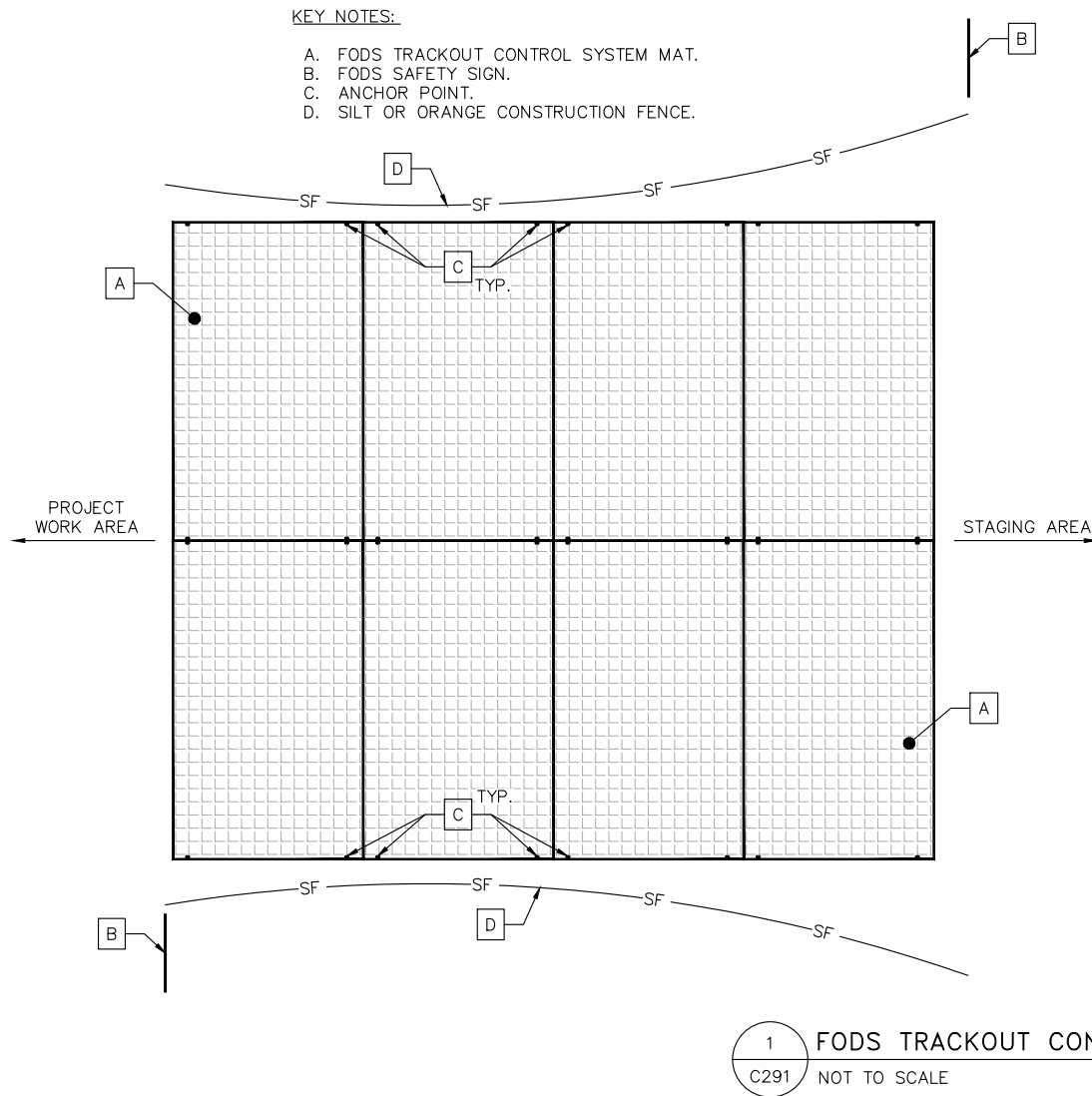
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EROSION CONTROL DETAILS

C290

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INSTALLATION:

1. THE SITE WHERE THE FODS TRACKOUT CONTROL SYSTEM IS TO BE PLACED SHOULD CORRESPOND TO BEST MANAGEMENT PRACTICES AS MUCH AS POSSIBLE. THE SITE WHERE FODS TRACKOUT CONTROL SYSTEM IS PLACED SHOULD ALSO MEET OR EXCEED THE LOCAL JURISDICTION OR STORM WATER POLLUTION PREVENTION PLAN (SWPPP) REQUIREMENTS.
2. CALL FOR UTILITY LOCATES 3 BUSINESS DAYS IN ADVANCE OF THE OF FODS TRACKOUT CONTROL SYSTEM INSTALLATION FOR THE MARKING OF UNDERGROUND UTILITIES. CALL THE UTILITY NOTIFICATION CENTER AT 811.
3. ONCE THE SITE IS ESTABLISHED WHERE FODS TRACKOUT CONTROL SYSTEM IS TO BE PLACED, ANY EXCESSIVE UNEVEN TERRAIN SHOULD BE LEVELED OUT OR REMOVED SUCH AS LARGE ROCKS, LANDSCAPING MATERIALS, OR SUDDEN ABRUPT CHANGES IN ELEVATION.
4. THE FIRST MAT SHOULD BE PLACED NEXT TO THE CLOSEST POINT OF EGRESS. THIS WILL ENSURE THAT THE VEHICLE WILL EXIT STRAIGHT FROM THE SITE ONTO THE PAVED SURFACE.
5. AFTER THE FIRST MAT IS PLACED DOWN IN THE PROPER LOCATION, MATS SHOULD BE ANCHORED TO PREVENT THE POTENTIAL MOVEMENT WHILE THE ADJOINING MATS ARE INSTALLED. ANCHORS SHOULD BE PLACED AT EVERY ANCHOR POINT (IF FEASIBLE) TO HELP MAINTAIN THE MAT IN ITS CURRENT POSITION.
6. AFTER THE FIRST MAT IS ANCHORED IN ITS PROPER PLACE, AN H BRACKET SHOULD BE PLACED AT THE END OF THE FIRST MAT BEFORE ANOTHER MAT IS PLACED ADJACENT TO THE FIRST MAT.
7. ONCE THE SECOND MAT IS PLACED ADJACENT TO THE FIRST MAT, MAKE SURE THE H BRACKET IS CORRECTLY SITUATED BETWEEN THE TWO MATS, AND SLIDE MATS TOGETHER.
8. NEXT THE CONNECTOR STRAPS SHOULD BE INSTALLED TO CONNECT THE TWO MATS TOGETHER.
9. UPON PLACEMENT OF EACH NEW MAT IN THE SYSTEM, THAT MAT SHOULD BE ANCHORED AT EVERY ANCHOR POINT TO HELP STABILIZE THE MAT AND ENSURE THE SYSTEM IS CONTINUOUS WITH NO GAPS IN BETWEEN THE MATS.
10. SUCCESSIVE MATS CAN THEN BE PLACED TO CREATE THE FODS TRACKOUT CONTROL SYSTEM REPEATING THE ABOVE STEPS.

USE AND MAINTENANCE

11. VEHICLES SHOULD TRAVEL DOWN THE LENGTH OF THE TRACKOUT CONTROL SYSTEM AND NOT CUT ACROSS THE MATS.
12. DRIVERS SHOULD TURN THE WHEEL OF THEIR VEHICLES SUCH THAT THE VEHICLE WILL MAKE A SHALLOW S-TURN ROUTE DOWN THE LENGTH OF THE FODS TRACKOUT CONTROL SYSTEM.
13. MATS SHOULD BE CLEANED ONCE THE VOIDS BETWEEN THE PYRAMIDS BECOME FULL OF SEDIMENT. TYPICALLY THIS WILL NEED TO BE PERFORMED WITHIN TWO WEEKS AFTER A STORM EVENT. BRUSHING IS THE PREFERRED METHOD OF CLEANING, EITHER MANUALLY OR MECHANICALLY.

REMOVAL

14. REMOVAL OF FODS TRACKOUT CONTROL SYSTEM IS REVERSE ORDER OF INSTALLATION.
15. STARTING WITH THE LAST MAT, THE MAT THAT IS PLACED AT THE INNERMOST POINT OF THE SITE OR THE MAT FURTHEST FROM THE EXIT OR PAVED SURFACE SHOULD BE REMOVED FIRST.
16. THE ANCHORS SHOULD BE REMOVED.
17. THE CONNECTOR STRAPS SHOULD BE UNBOLTED AT ALL LOCATIONS IN THE FODS TRACKOUT CONTROL SYSTEM.
18. STARTING WITH THE LAST MAT IN THE SYSTEM, EACH SUCCESSIVE MAT SHOULD THEN BE MOVED AND STACKED FOR LOADING BY FORKLIFT OR EXCAVATOR ONTO A TRUCK FOR REMOVAL FROM THE SITE.

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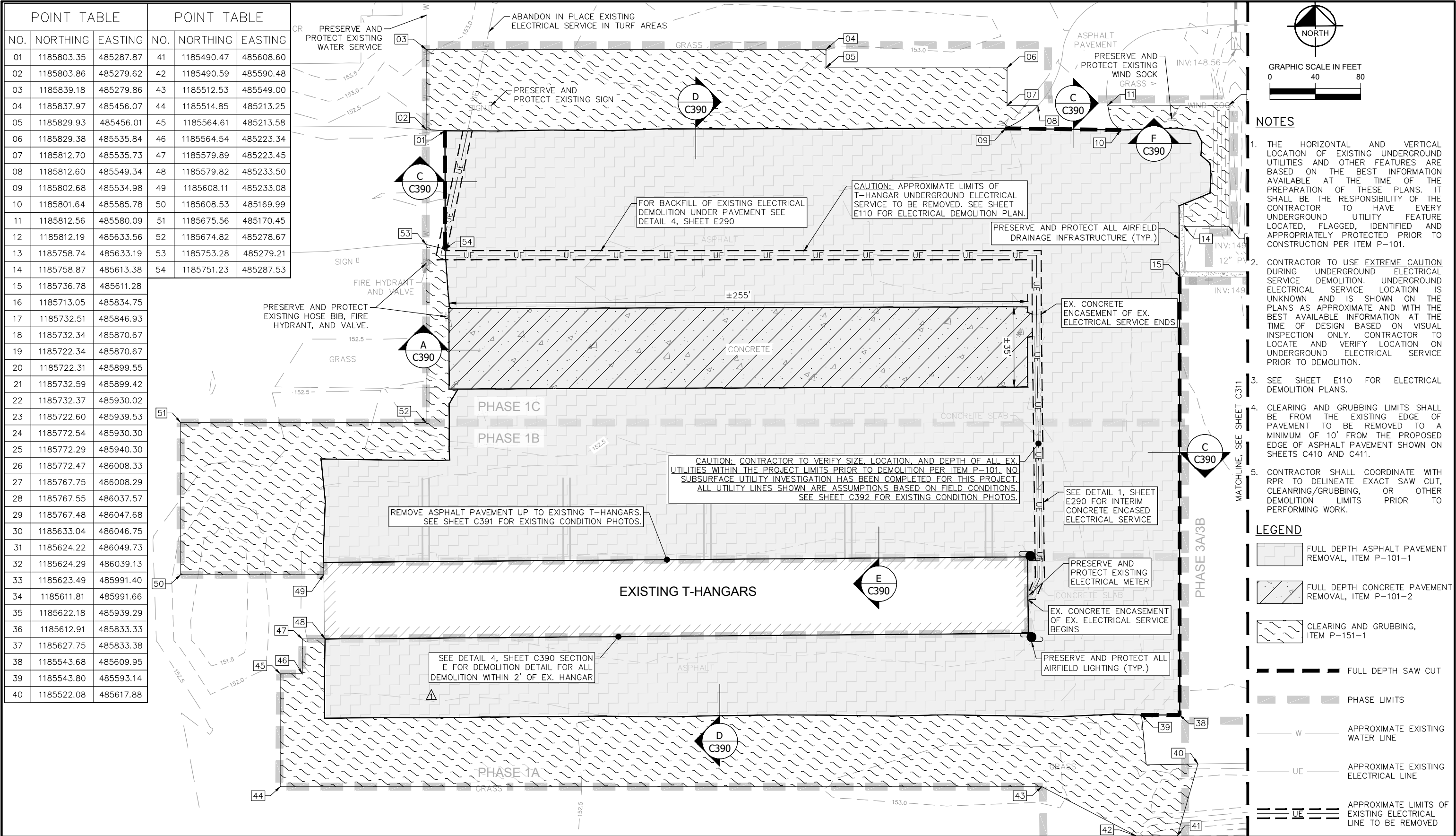
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EROSION CONTROL DETAILS

C291



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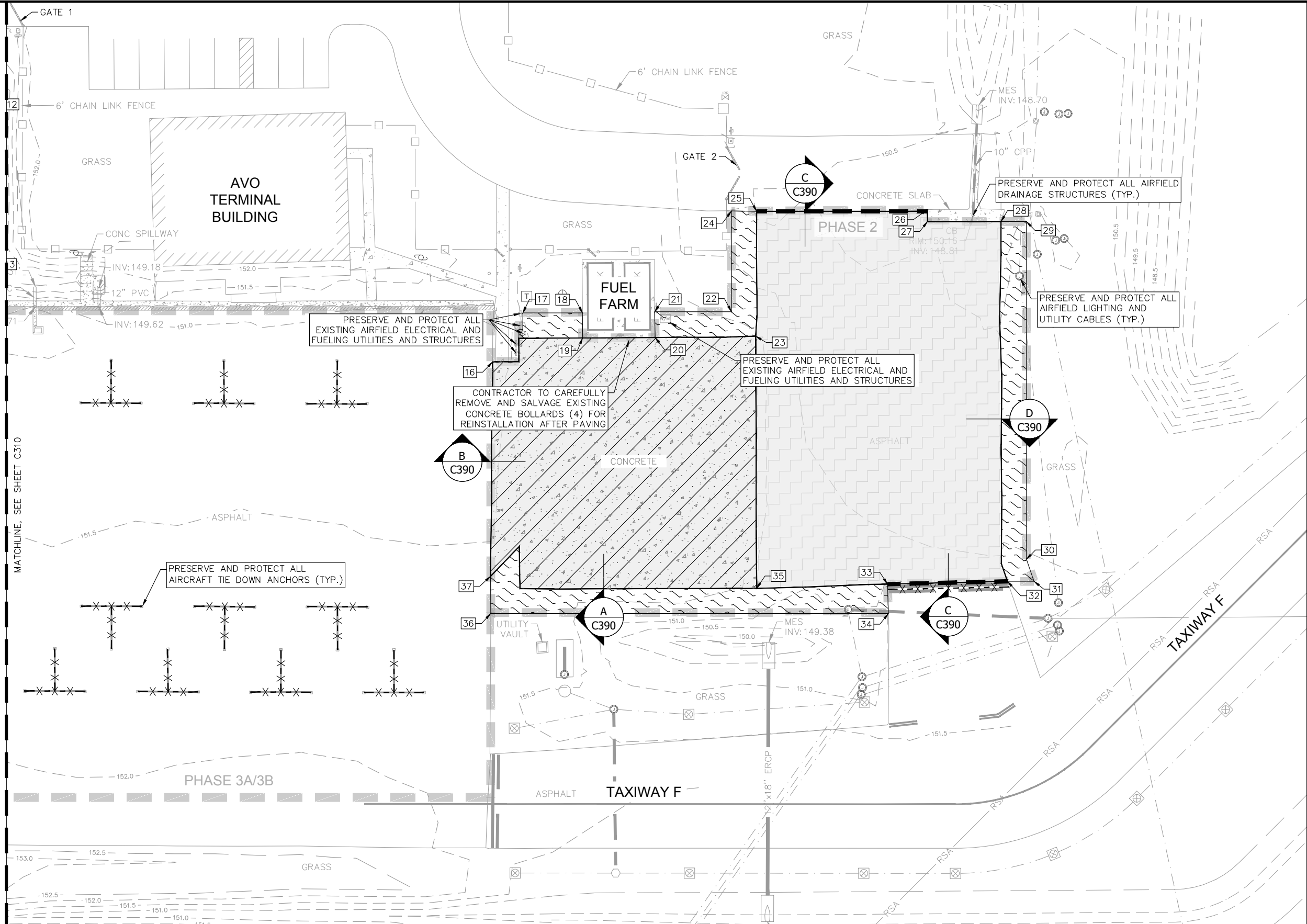
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DEMOLITION PLANS

C310

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NOTES

1. SEE SHEET C310 FOR DEMOLITION NOTES AND POINT TABLE.

LEGEND

- FULL DEPTH ASPHALT PAVEMENT REMOVAL, ITEM P-101-1
- FULL DEPTH CONCRETE HANGAR SLAB REMOVAL, ITEM P-101-2
- CLEARING AND GRUBBING, ITEM P-151-1
- FULL DEPTH SAW CUT
- PHASE LIMITS
- MARKING REMOVAL

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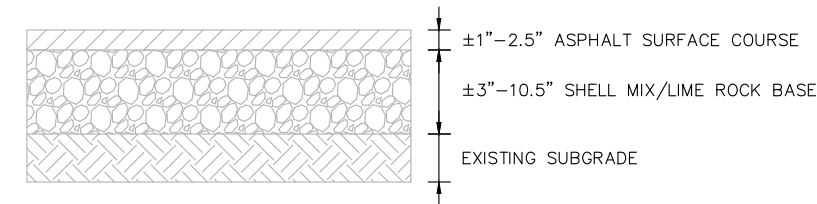
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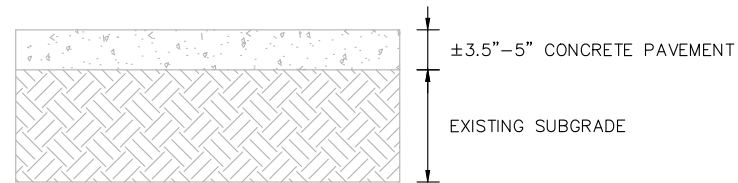
DEMOLITION PLANS

C311





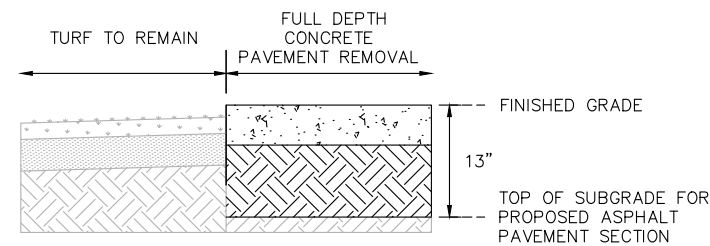
1 EXISTING APRON ASPHALT PAVEMENT TYPICAL SECTION  
C390



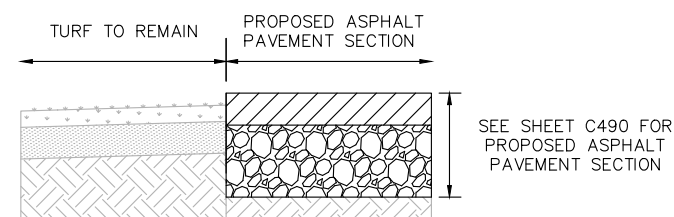
2 EXISTING APRON CONCRETE PAVEMENT TYPICAL SECTION  
C390

NOTES:

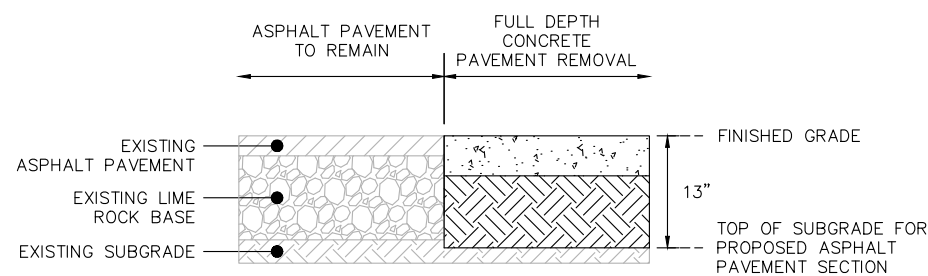
1. THE THICKNESS OF EXISTING MATERIALS ARE BASED OFF OF GEOTECHNICAL INVESTIGATIONS IN THE PROJECT AREA AND THE BEST AVAILABLE DATA. THE EXISTING MATERIALS TO REMAIN ARE SHOWN FOR REFERENCE ONLY AND MAY VARY FROM THE THICKNESSES SHOWN.
2. FOR GEOTECHNICAL INVESTIGATIVE DATA SEE SHEETS B110-B113.



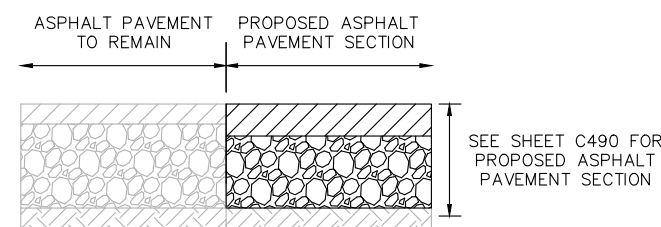
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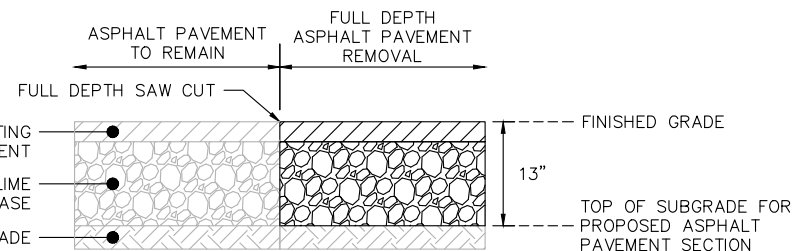
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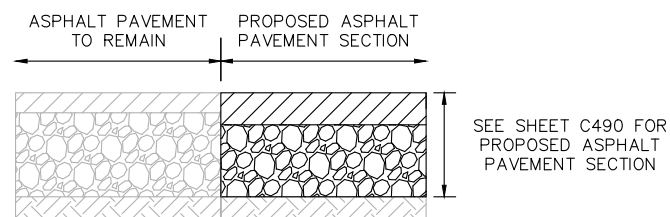
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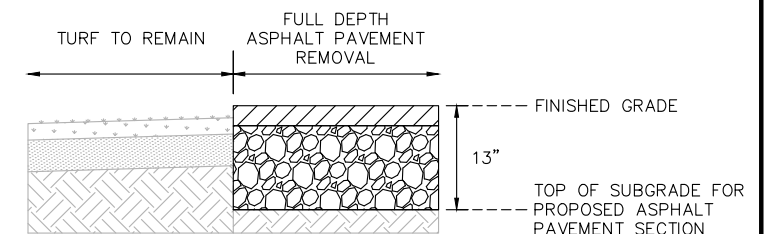
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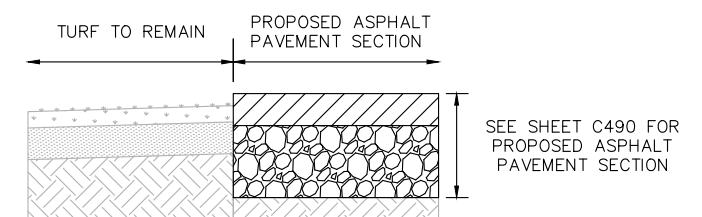
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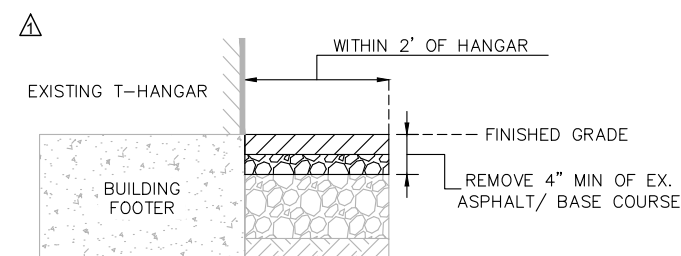
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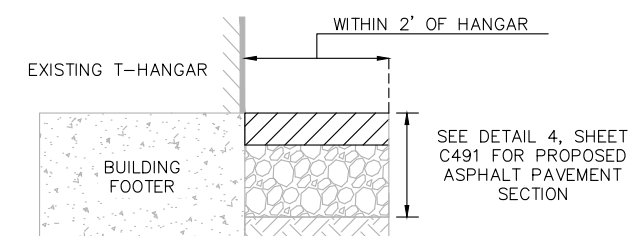
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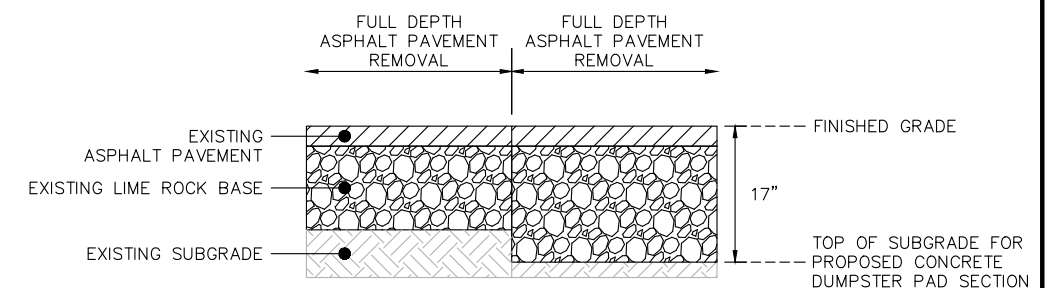
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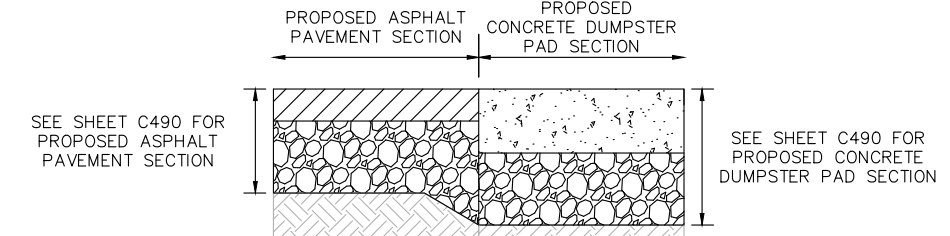
SECTION I



SECTION J



SECTION K



SECTION L

3 FULL DEPTH CONCRETE PAVEMENT REMOVAL  
C390

4 FULL DEPTH ASPHALT PAVEMENT REMOVAL  
C390

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DEMOLITION DETAILS

C390



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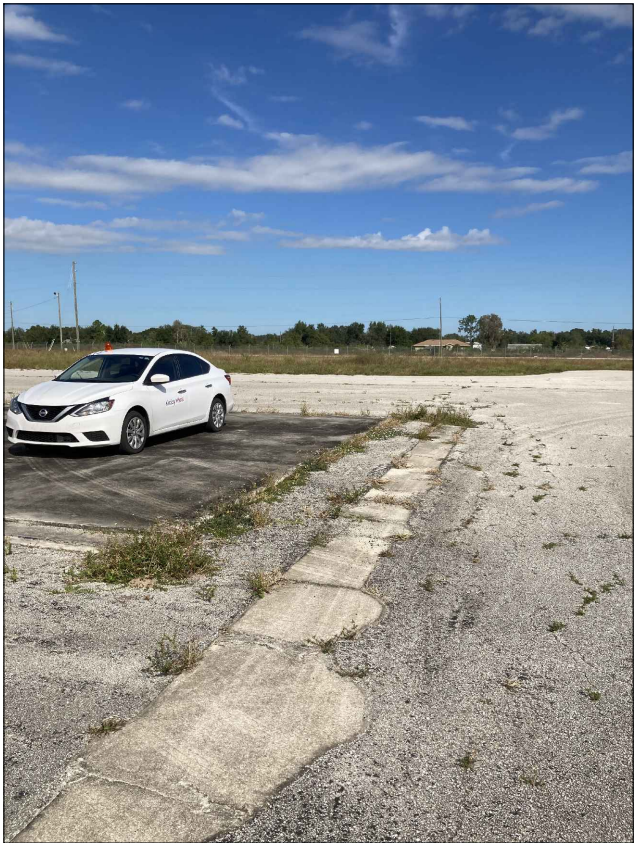
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EXISTING SITE PHOTOS

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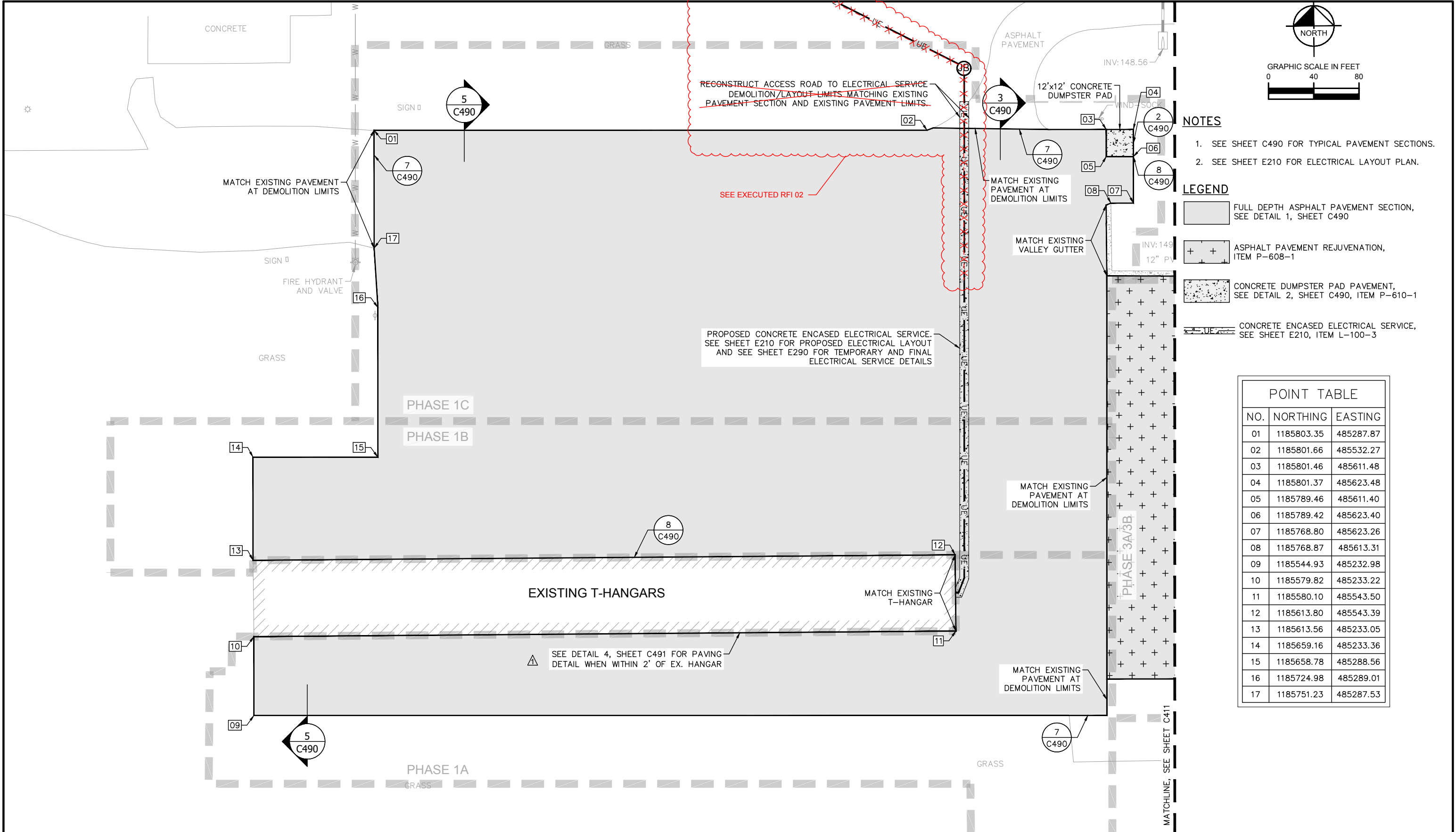
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NOTES


- 1. SEE SHEET C490 FOR TYPICAL PAVEMENT SECTIONS.
- 2. SEE SHEET E210 FOR ELECTRICAL LAYOUT PLAN.

LEGEND

- FULL DEPTH ASPHALT PAVEMENT SECTION, SEE DETAIL 1, SHEET C490
- ASPHALT PAVEMENT REJUVENATION, ITEM P-608-1
- CONCRETE DUMPSTER PAD PAVEMENT, SEE DETAIL 2, SHEET C490, ITEM P-610-1
- CONCRETE ENCASED ELECTRICAL SERVICE, SEE SHEET E210, ITEM L-100-3

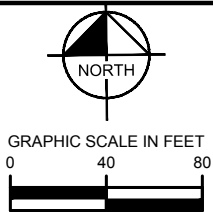
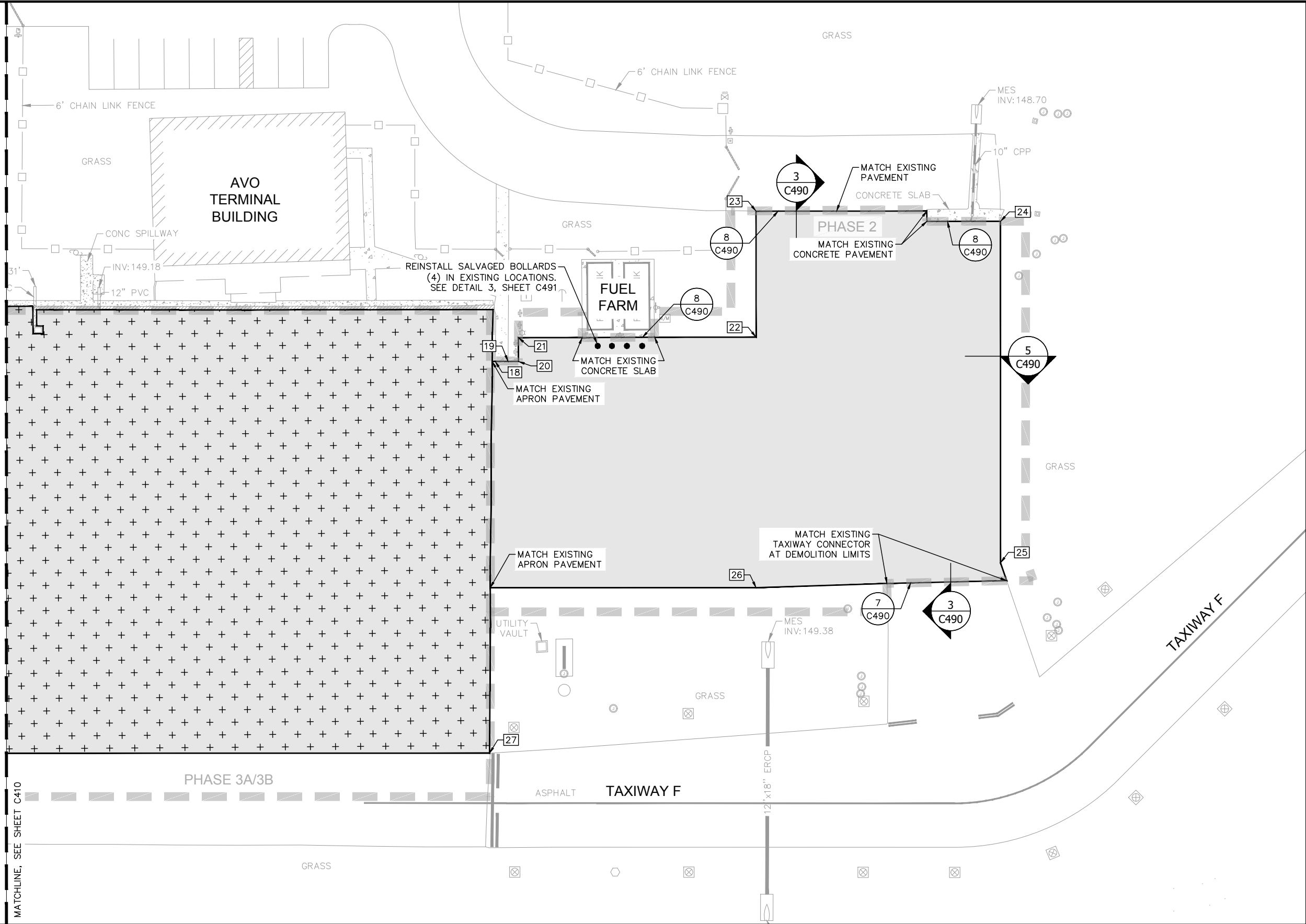
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04	1185801.37	485623.48
05	1185789.46	485611.40
06	1185789.42	485623.40
07	1185768.80	485623.26
08	1185768.87	485613.31
09	1185544.93	485232.98
10	1185579.82	485233.22
11	1185580.10	485543.50
12	1185613.80	485543.39
13	1185613.56	485233.05
14	1185659.16	485233.36
15	1185658.78	485288.56
16	1185724.98	485289.01
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- NOTES**
1. SEE SHEET C490 FOR TYPICAL PAVEMENT SECTIONS.

- LEGEND**
- FULL DEPTH ASPHALT PAVEMENT SECTION, SEE DETAIL 1, SHEET C490
  - ASPHALT PAVEMENT REJUVENATION, ITEM P-608-1

POINT TABLE		
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21	1185722.52	485845.24
22	1185722.03	485939.95
23	1185772.29	485940.30
24	1185767.55	486037.68
25	1185631.45	486036.74
26	1185622.18	485939.29
27	1185556.95	485832.62

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FBO/TERMINAL APRON  
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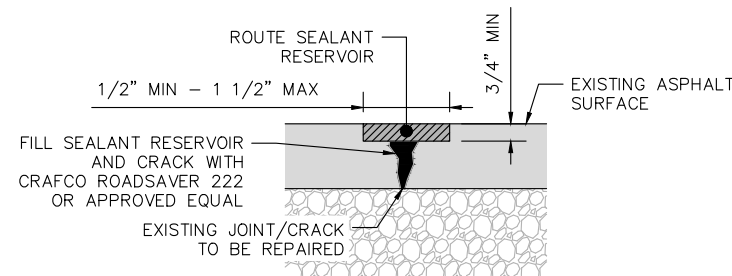
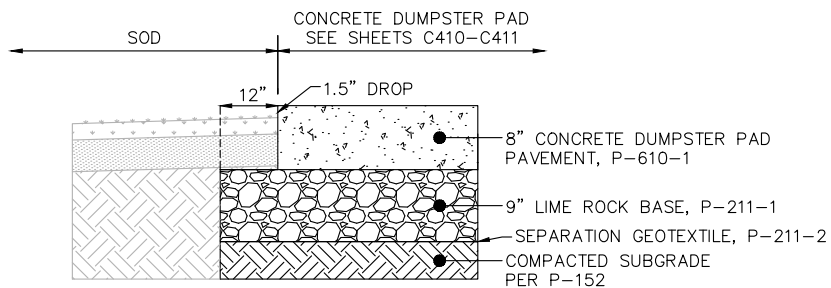
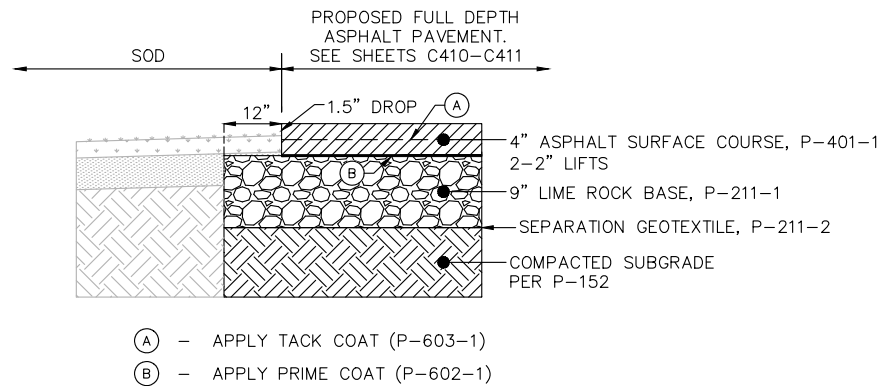
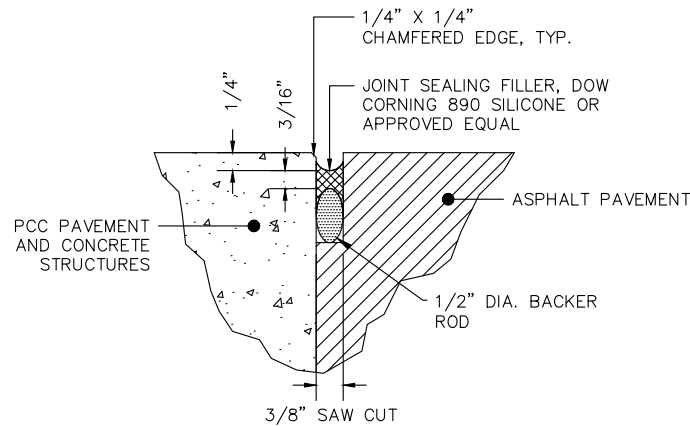
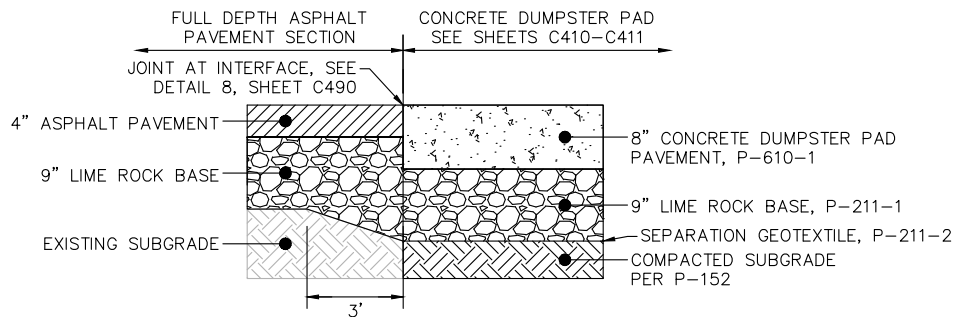
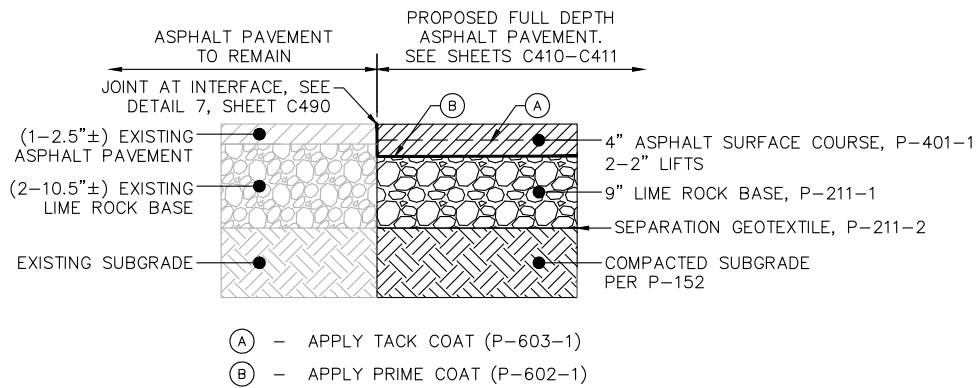
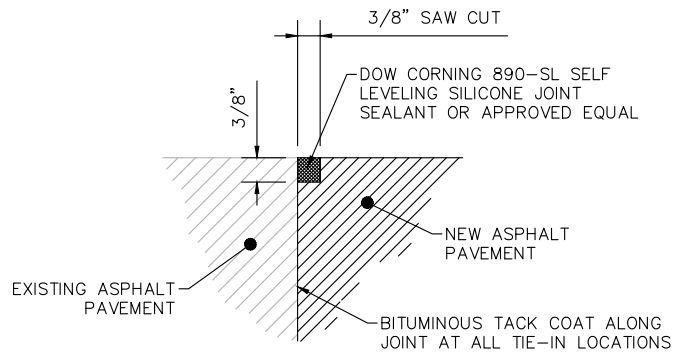
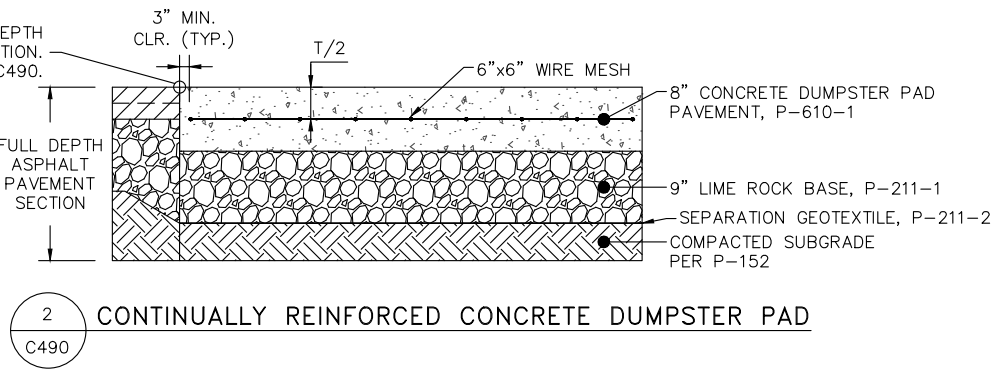
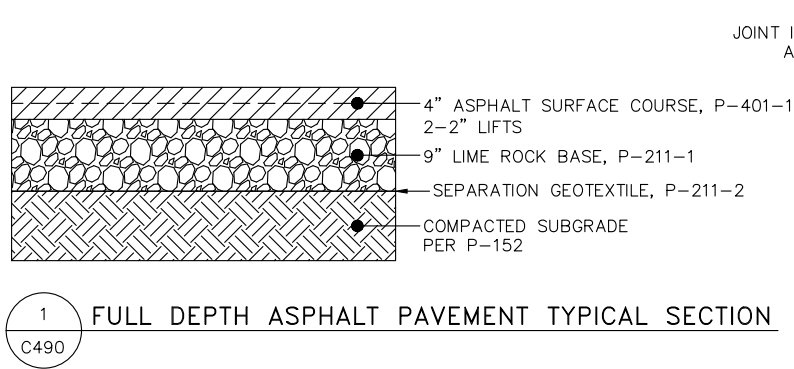
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GEOMETRY AND PAVING PLANS

**C411**

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NOTE:

1. ALL CRACKS 1/4" OR WIDER AFTER MILLING OPERATIONS SHALL BE ROUTED AND SEALED BEFORE PLACEMENT OF THE ASPHALT OVERLAY

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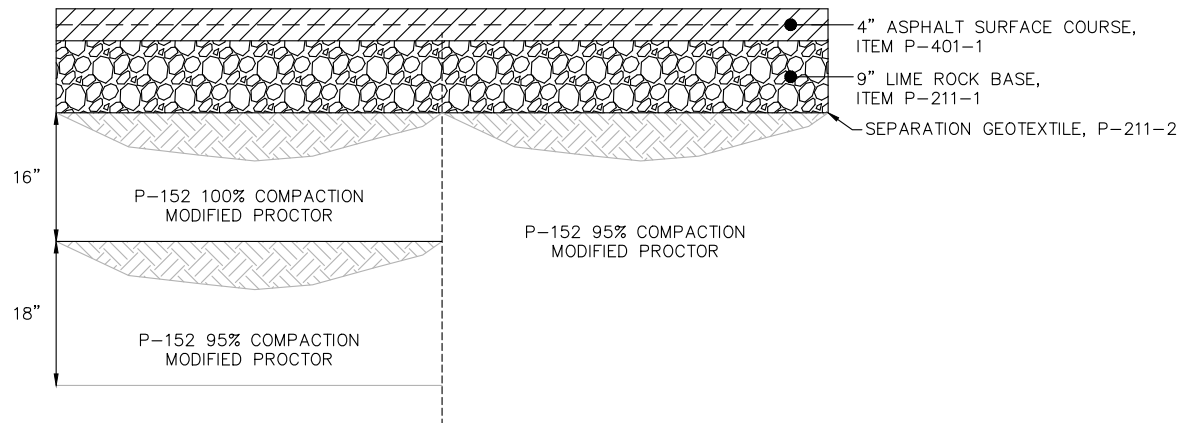
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GEOMETRY AND PAVING DETAILS

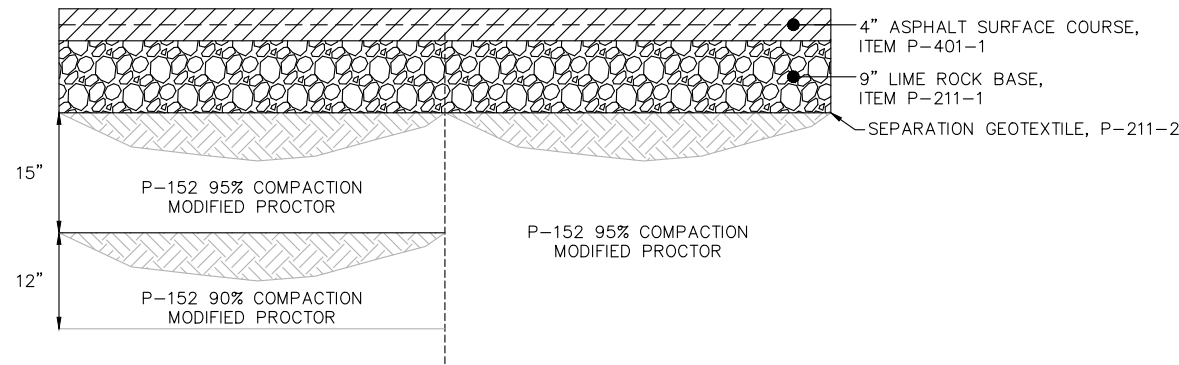
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FILL SECITON

CUT SECTION

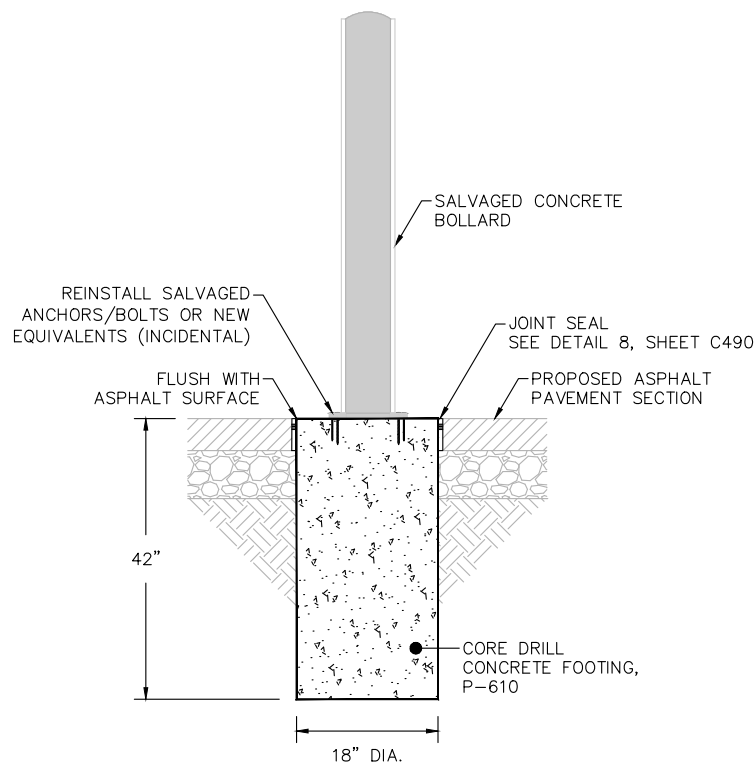
1 FULL DEPTH ASPHALT PAVEMENT COMPACTION SCHEDULE  
NON-COHESIVE SOILS  
C491



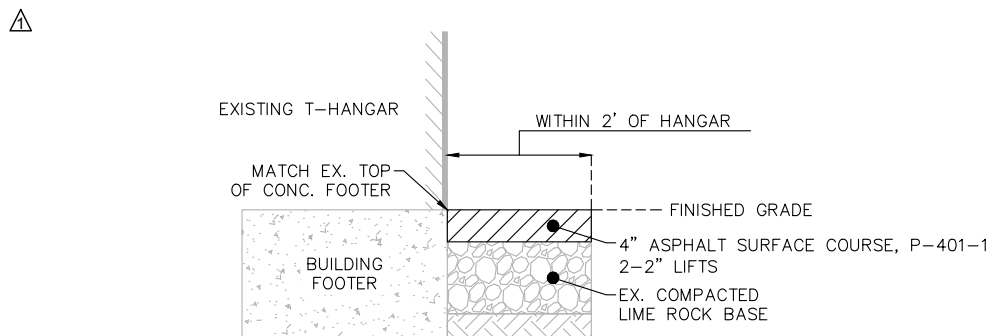
FILL SECITON

CUT SECTION

2 FULL DEPTH ASPHALT PAVEMENT COMPACTION SCHEDULE  
COHESIVE SOILS  
C491



3 SALVAGED CONCRETE BOLLARD INSTALLATION  
C491 NOT TO SCALE



NOTE:

1. CONTRACTOR SHALL PROVIDE COMPACTION (MACHINE OR HAND) FOR DISTURBED EX. LIME ROCK BASE WHEN WITHIN 2' OF EX. HANGAR TO PROVIDE A SUITABLE PAVING SURFACE.

4 ASPHALT PAVING DETAIL WHEN WITHIN 2' OF HANGAR  
C491 NOT TO SCALE

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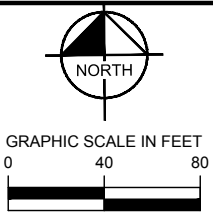
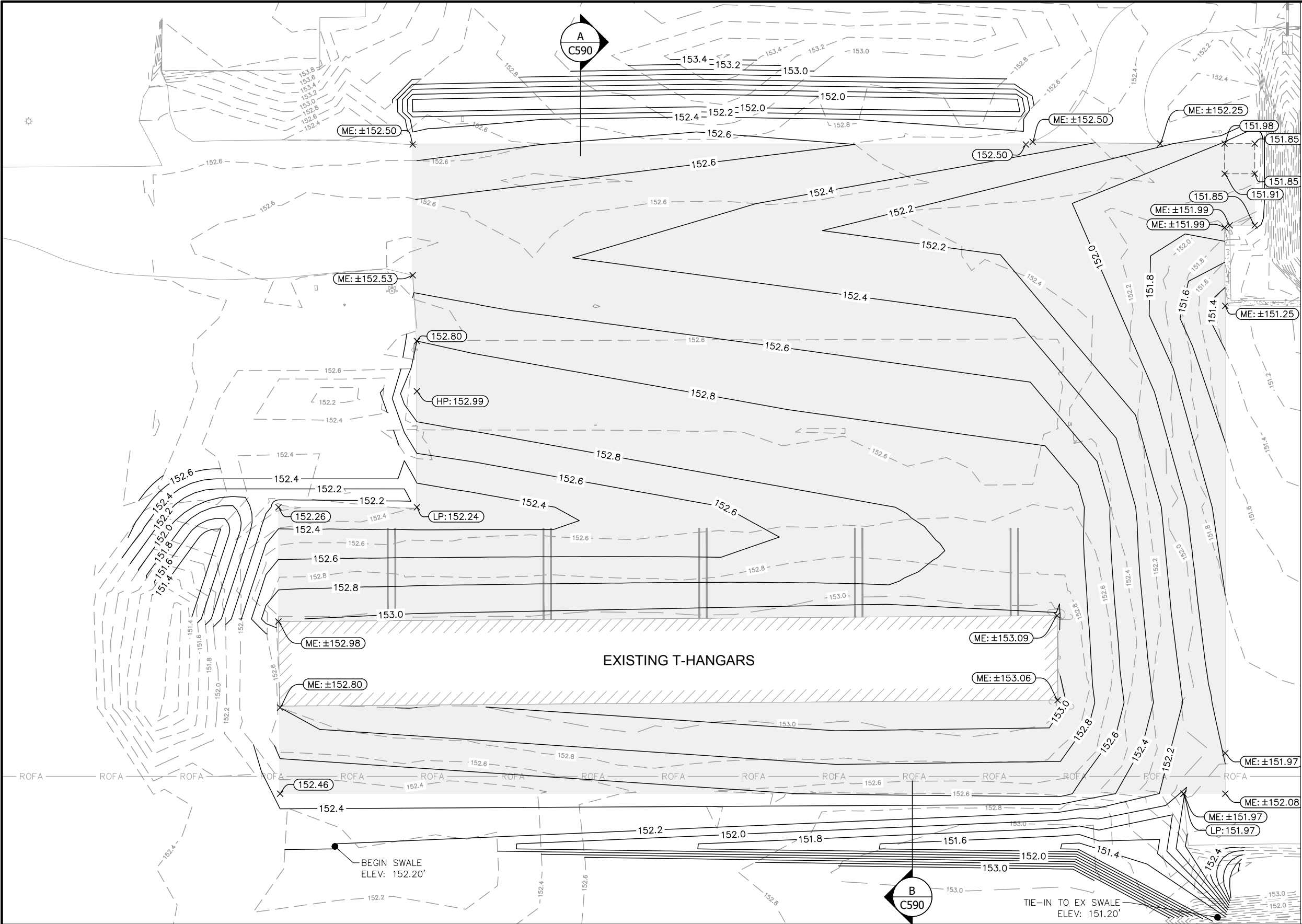
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GEOMETRY AND PAVING DETAILS

C491



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**NOTES**

1. SEE SHEET C410 & C411 FOR PAVEMENT GEOMETRY.

- LEGEND**
- PROPOSED PAVEMENT
  - MATCH EXISTING ELEVATION (ME: ±152.20)
  - LOW POINT ELEVATION (LP: 152.45)
  - HIGH POINT ELEVATION (HP: 152.45)
  - PROPOSED SPOT ELEVATION (152.60)
  - PROPOSED CONTOUR (152.0)
  - EXISTING CONTOUR (152.0)

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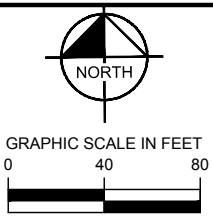
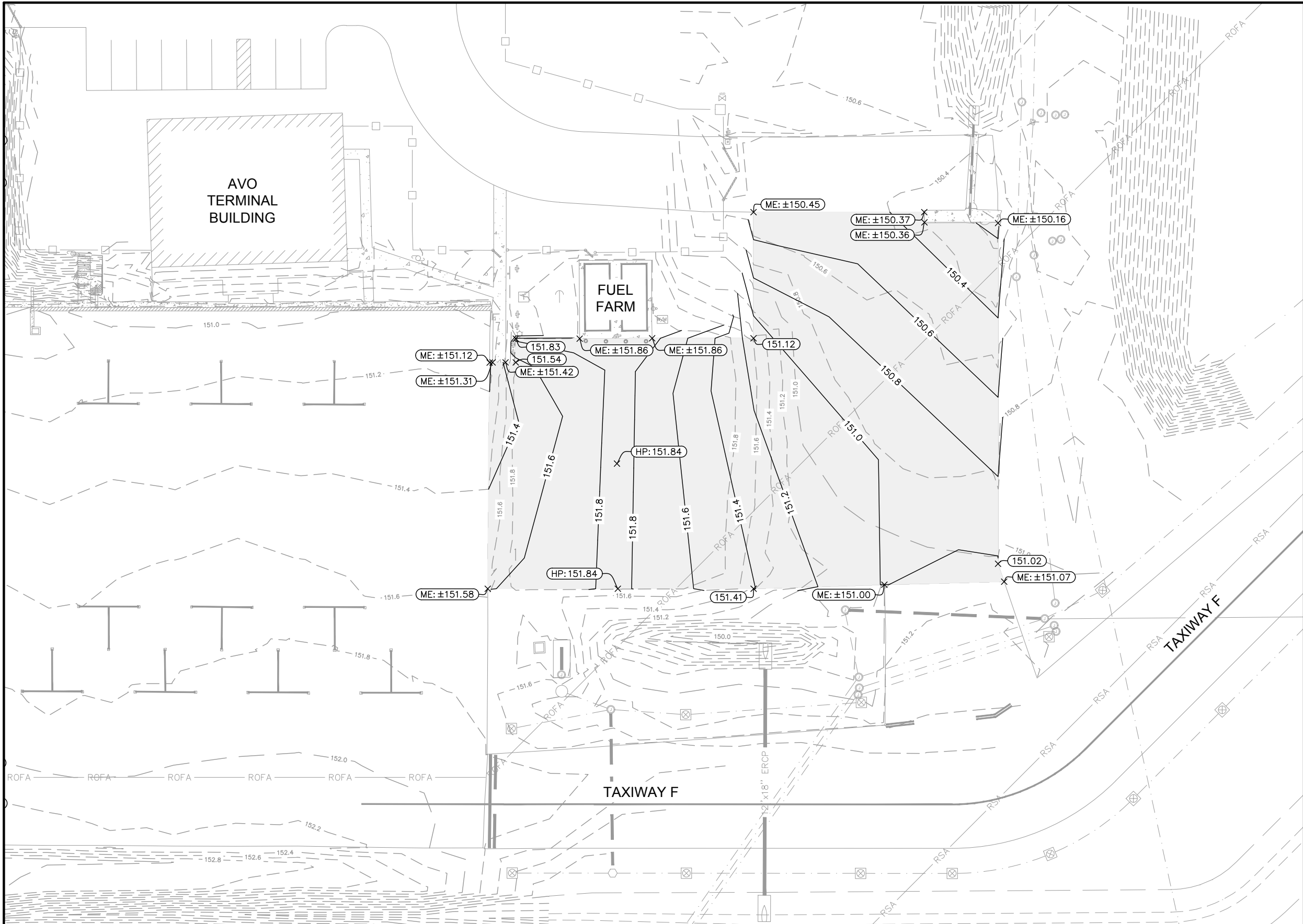
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GRADING PLANS

C510

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- NOTES**
- SEE SHEET C410 & C411 FOR PAVEMENT GEOMETRY.
- LEGEND**
- PROPOSED ASPHALT PAVEMENT
  - MATCH EXISTING ELEVATION
  - PROPOSED SPOT ELEVATION
  - PROPOSED CONTOUR
  - EXISTING CONTOUR

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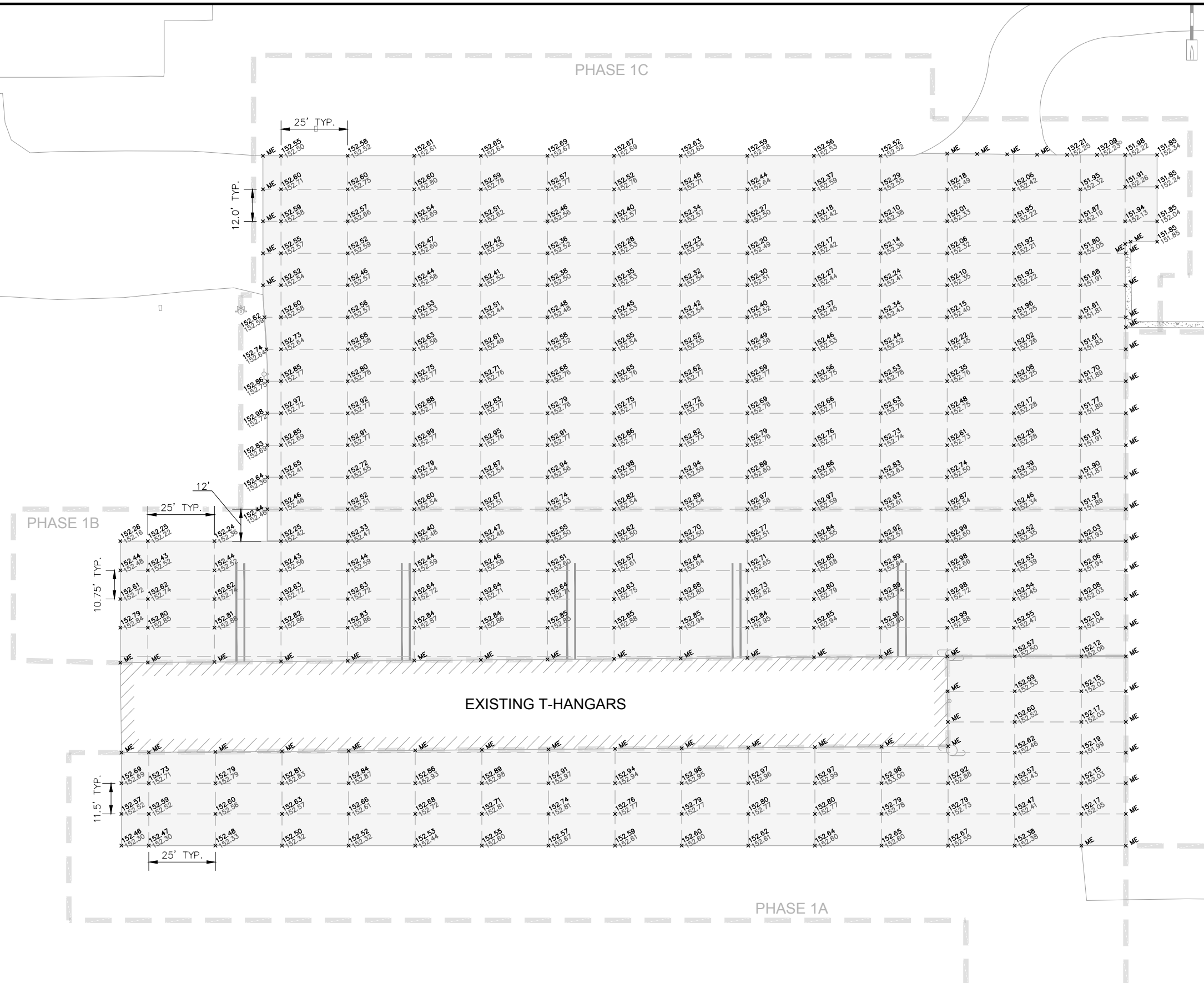
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GRADING PLANS

C511

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LEGEND

- PROPOSED PAVEMENT
- PHASE LIMITS
- PAVEMENT SPOT ELEVATION
- MATCH EXISTING ELEVATION

NOTE: SEE AS-BUILT SURVEY PROVIDED BY AJAX PAVING INDUSTRIES OF FLORIDA AND COMPLETED BY A.A. SURFACE PRO FOR FINAL PAVEMENT SPOT ELEVATIONS.

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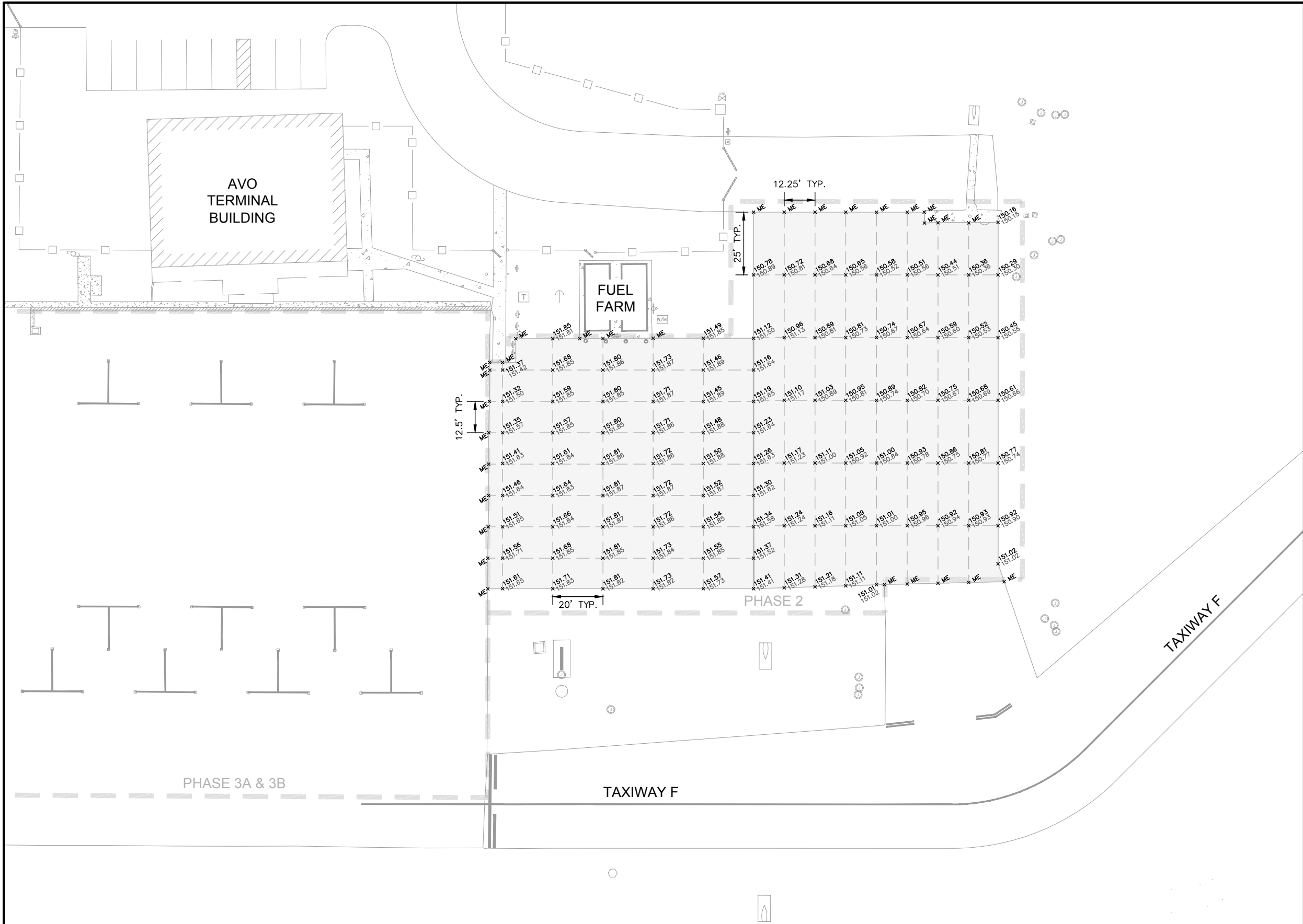
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PAVEMENT ELEVATION PLAN

C520



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**LEGEND**

- PROPOSED PAVEMENT
- PHASE LIMITS
- PAVEMENT SPOT ELEVATION
- MATCH EXISTING ELEVATION

**NOTE: SEE AS-BUILT SURVEY PROVIDED BY AJAX PAVING INDUSTRIES OF FLORIDA AND COMPLETED BY A.A. SURFACE PRO FOR FINAL PAVEMENT SPOT ELEVATIONS.**

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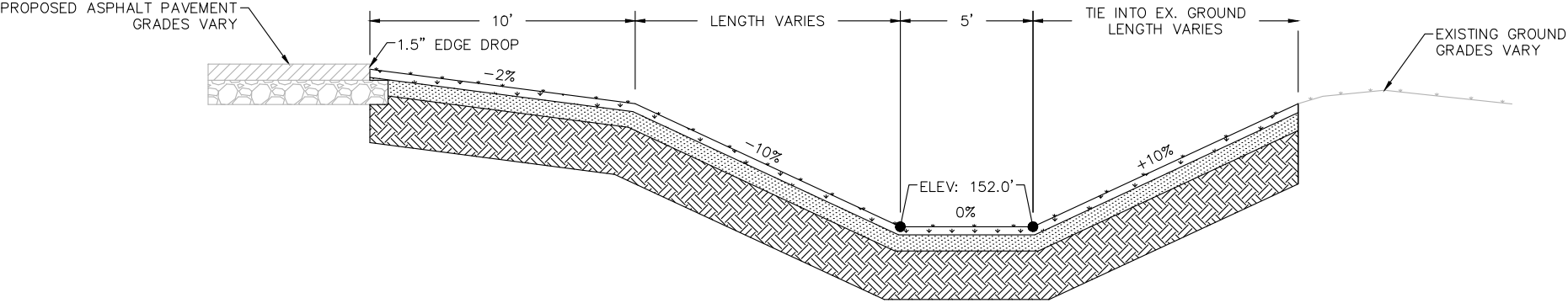
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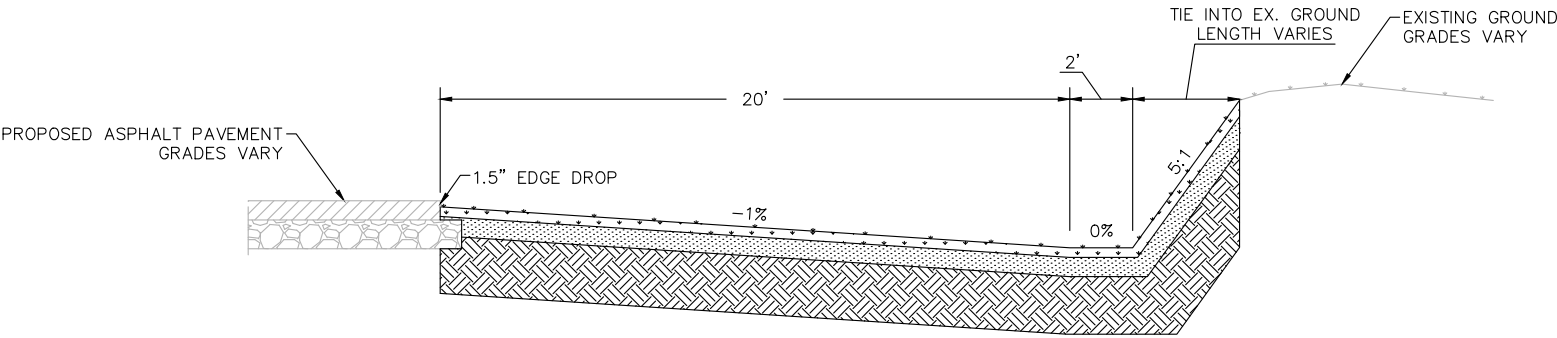
**PAVEMENT ELEVATION PLAN**

**C521**

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SECTION A



SECTION B

1 TYPICAL TURF SECTIONS  
C590 NOT TO SCALE

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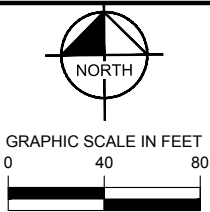
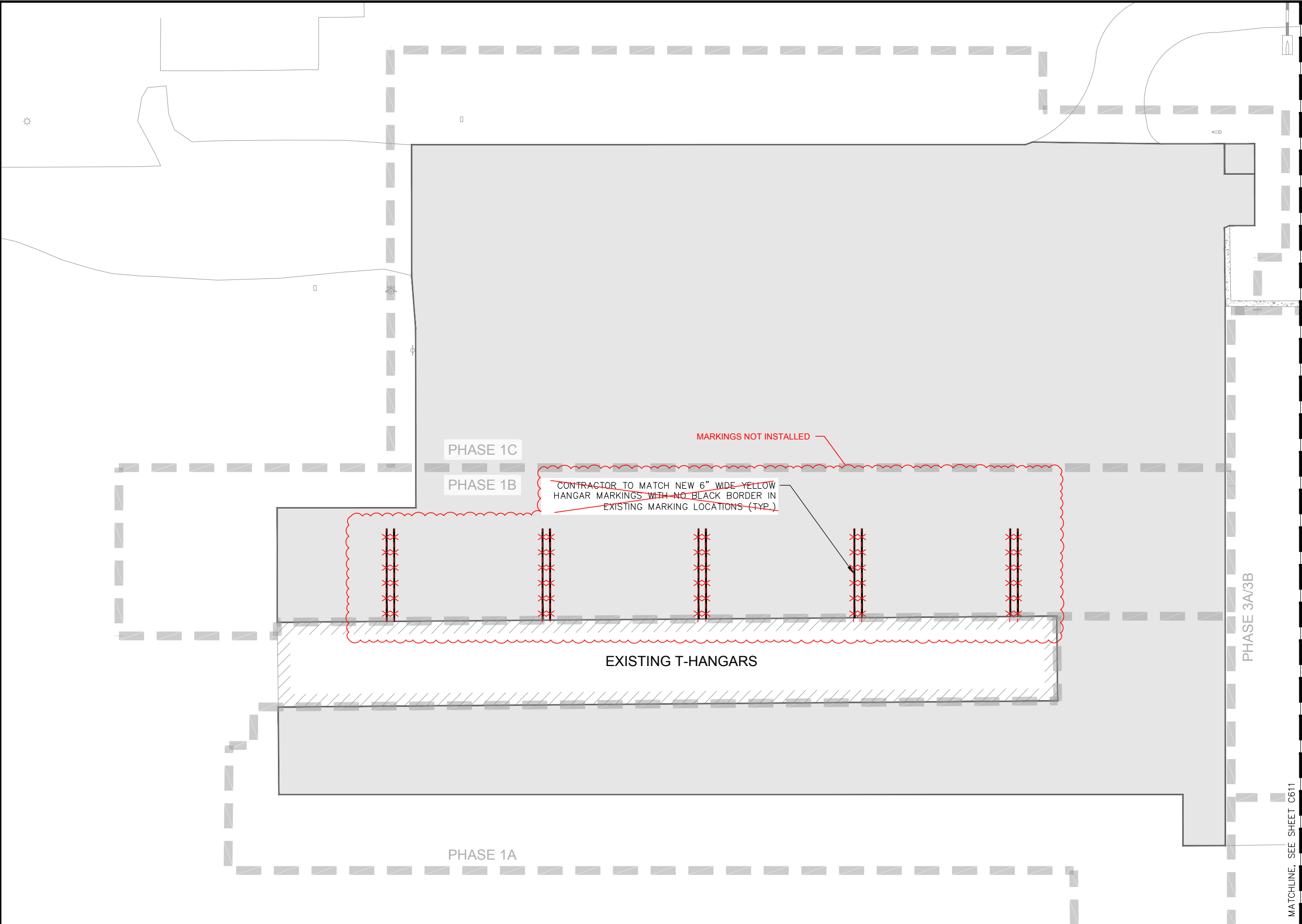
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GRADING DETAILS

C590

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NOTES

1. ALL AIRFIELD MARKING SHALL BE IN ACCORDANCE WITH FAA ADVISORY CIRCULAR AC 150/5340-1M, "STANDARDS FOR AIRPORT MARKING" OR LATEST VERSION OF THE AC.
2. PAINT REMOVAL SHALL BE ACCOMPLISHED PER SPECIFICATION P-620.
3. CONTRACTOR SHALL LAYOUT ALL PROPOSED MARKINGS PRIOR TO ANY PAINTING FOR INSPECTION BY THE RPR. AFTER LAYOUT, THE RPR MAY REQUIRE ADJUSTMENTS IN THE FIELD PRIOR TO APPLYING THE MARKINGS. ANY ADDITIONAL LAYOUT SHALL BE INCIDENTAL TO ITEM P-620.
4. HAND SPRAYING OR ROLLER PAINTING IS NOT ACCEPTABLE. SEE SPECIFICATION P-620 FOR ACCEPTABLE METHODS OF APPLICATION.
5. ALL PAINT SHALL BE WATERBORNE PAINT AND SHALL MEET THE REQUIREMENTS OF TT-P-1952.
6. ALL MARKINGS SHALL MATCH EXISTING MARKINGS AT THE LIMIT OF CONSTRUCTION. CONTRACTOR AND RPR ARE TO FIELD VERIFY LOCATION OF TIE-IN POINT TO EXISTING MARKING.
7. ALL RETROREFLECTIVE PAINT SHALL HAVE GLASS BEADS, SEE SPECIFICATION P-620 FOR TYPE.

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AVON PARK, FL

**Kimley»Horn**  
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655 NORTH FRANKLIN ST., SUITE 150  
TAMPA, FLORIDA 33602  
PHONE: 813-620-1460  
WWW.KIMLEY-HORN.COM REGISTRY NO. 35106

JOB NO:	046464008
DRAWN:	BJG
DESIGN:	BJG
CHECKED:	JCM
DATE:	3/10/2022

PAVEMENT MARKING PLANS

C610



REVISIONS:			
REV. NO	DATE	DESCRIPTION:	EOR
	12/12/2022	CONFORMED DOCUMENTS	JCM
	10/12/2023	RECORD DRAWINGS	JCM



FBO/TERMINAL APRON  
REHABILITATION  
CITY OF AVON PARK  
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PAVEMENT MARKING PLANS
C611

K:\TAM\_Aviation\1\_Engr\_AVO\2019 - On Call Engineering\Task Orders\FY 2021 - Apron Rehab\CAD\Plan Sheets\ELECTRICAL.dwg Tab: E001 Oct 12, 2023 - 10:08am Plotted by: katie.dodge

GENERAL ELECTRICAL NOTES:

1. THE CONTRACTOR SHALL FURNISH ALL LABOR, MATERIAL, EQUIPMENT, AND TRANSPORTATION NECESSARY TO CONSTRUCT ALL ELEMENTS OF THE PROJECT ELECTRICAL WORK AS DESCRIBED IN THE FOLLOWING PLANS, INCLUDING BUT NOT LIMITED TO:
  - 1.1. REMOVAL OF EXISTING ELECTRICAL SERVICE AS INDICATED ON THE ELECTRICAL DEMOLITION SHEETS.
  - 1.2. INSTALLATION OF NEW ELECTRICAL SERVICE AND JUNCTION CAN AS INDICATED ON THE ELECTRICAL LAYOUT SHEETS.
2. THE PROJECT PAY ITEMS ARE INTENDED TO BE INCLUSIVE OF ALL WORK TO BE PERFORMED AS SHOWN IN THESE PLANS. ALL INCIDENTAL WORK REQUIRED TO COMPLETE THE PROJECT ELECTRICAL WORK, TO THE SATISFACTION OF THE RPR, SHALL BE INCLUDED IN THE COSTS OF PERFORMING THESE PAY ITEMS.
3. ALL WORK AND MATERIAL, AS A MINIMUM, SHALL BE AS PER THE LATEST FAA ADVISORY CIRCULAR REGARDING THAT PORTION OF WORK.
4. THE CONTRACTOR SHALL PAY CLOSE ATTENTION TO THE SAFETY AND PHASING PLANS AND THE AIRPORT SAFETY REQUIREMENTS SECTION OF THE SPECIFICATIONS. THESE SHALL BE STRICTLY ENFORCED.
5. UPON COMPLETION OF THE WORK, THE CONTRACTOR SHALL REMOVE FROM THE SITE ALL SURPLUS MATERIAL AND EQUIPMENT BELONGING TO THE CONTRACTOR AND THEIR SUBCONTRACTORS.
6. THE CONTRACTOR SHALL EXERCISE EXTREME CAUTION WHEN EXCAVATING IN AREAS OF EXISTING UTILITIES. THE CONTRACTOR SHALL HAVE EXISTING UTILITIES LOCATED AND MARKED IN ADVANCE OF EXCAVATION IN ALL AREAS. ALL DAMAGE TO EXISTING UTILITIES TO REMAIN WILL BE REPAIRED IMMEDIATELY TO THE SATISFACTION OF THE RPR. COST OF REPAIRS SHALL BE BORNE BY THE CONTRACTOR. THE LOCATION OF ANY UTILITIES SHOWN ON THE PLANS IS APPROXIMATE ONLY. DEPTH ARE NOT KNOWN. THE CONTRACTOR SHALL HAND EXCAVATE WHEN EXCAVATING OR TRENCHING WITHIN THREE FEET OF KNOWN OR SUSPECTED UNDERGROUND UTILITIES TO REMAIN.
7. CONTRACTOR TO LOCATE AND PROTECT EXISTING UTILITIES AND FACILITIES FROM DAMAGE DURING PERFORMANCE OF THE WORK. THE CONTRACTOR WILL CONTACT ALL UTILITY AND FACILITY AGENCIES FOR FIELD MARKING PER FLORIDA ONE-CALL PROCEDURES, PRIOR TO BEGINNING ANY CONSTRUCTION. NOT ALL UTILITIES MAY BE SHOWN. THE CONTRACTOR WILL DETERMINE THE EXACT LOCATION OF ALL EXISTING UTILITIES BEFORE BEGINNING ANY WORK, AND AGREES TO BE FULLY RESPONSIBLE FOR DAMAGES TO UTILITIES OR FACILITIES TO REMAIN. ALL EXISTING UTILITIES AND UNDERGROUND UTILITIES AND ASSOCIATED APPURTENANCES ARE SHOWN SCHEMATICALLY AND REPRESENT THE BEST INFORMATION AT THE TIME OF PLAN PREPARATION AND ARE NOT INTENDED TO REPRESENT THE ACTUAL NUMBER OR LOCATION AND ARE FOR GENERAL INFORMATION ONLY. ANY INTERPRETATION OF AN EXISTING SYSTEM OR UTILITY SERVICE SHALL BE COORDINATED AND APPROVED BY THE AUTHORITY, AGENCY OR UTILITY HAVING JURISDICTION PRIOR TO THE START OF WORK.
8. ALL LIGHTING SYSTEM COMPONENTS FURNISHED BY THE CONTRACTOR (INCLUDING FAA EQUIPMENT, IF APPLICABLE) SHALL BE COMPATIBLE IN ALL RESPECTS WITH EACH OTHER AND THE EXISTING ELECTRICAL SYSTEM. ANY NON-COMPATIBLE COMPONENTS FURNISHED BY THE CONTRACTOR SHALL BE REPLACED AT CONTRACTOR'S EXPENSE WITH A SIMILAR UNIT APPROVED BY THE RPR THAT IS COMPATIBLE WITH THE EXISTING ELECTRICAL SYSTEM.
9. THE CONTRACTOR INSTALLED EQUIPMENT SHALL NOT GENERATE ANY ELECTROMAGNETIC INTERFERENCE WITH EXISTING AND/OR NEW COMMUNICATIONS, WEATHER, AIR NAVIGATION AND AIR TRAFFIC CONTROL EQUIPMENT. ANY EQUIPMENT GENERATING SUCH INTERFERENCE SHALL BE REPLACED BY THE CONTRACTOR AT CONTRACTOR'S EXPENSE WITH EQUIPMENT MEETING THE APPLICABLE SPECIFICATIONS AND NOT GENERATING ANY INTERFERENCE.
10. THE CONTRACTOR SHALL OBTAIN AND PAY FOR ALL REQUIRED PERMITS, LICENSES, ETC., PRIOR TO COMMENCEMENT OF WORK. THE COSTS OF PERMITS, LICENSES, ETC., SHALL BE INCIDENTAL TO AND INCLUDED IN THE BID PRICE FOR THE RESPECTIVE PAY ITEMS.
11. DEWATERING FOR THE INSTALLATION OF MANHOLES, HANDHOLES, AND DUCTBANKS IS INCIDENTAL TO THE RESPECTIVE PAY ITEM. THE CONTRACTOR SHALL BE RESPONSIBLE TO PAY FOR AND OBTAIN ANY AND ALL PERMITS REQUIRED FOR DEWATERING.
12. ALL EXISTING SYSTEMS TO REMAIN SHALL BE PROTECTED FROM DAMAGE.
13. FAA CABLES AND FIBER OPTIC CABLES DAMAGED DURING CONSTRUCTION CANNOT BE SPLICED BY THE CONTRACTOR UNLESS APPROVED BY THE FAA AND THE AIRPORT. DAMAGE TO ANY EXISTING CABLE SHALL RESULT IN THE

CONTRACTOR BEING RESPONSIBLE FOR THE COMPLETE REPLACEMENT OF THE CABLE.

14. CONTRACTOR SHALL MEG ALL CIRCUITS PRIOR TO START OF CONSTRUCTION IN THE PRESENCE OF THE RPR. AT THE END OF CONSTRUCTION, ALL CIRCUITS WILL BE RE-MEGGED IN THE PRESENCE OF THE RPR. WHEN MEG READINGS ARE TAKEN, THE CONTRACTOR SHALL ALSO DOCUMENT CURRENT AND RECENT WEATHER CONDITIONS. ALL MEG READINGS SHALL BE TAKEN AT SIMILAR WEATHER CONDITIONS. THE MEGGER USED SHALL BE A DIGITAL 500V OR 1000V DC MEGGER. ALL NEW CIRCUITS SHALL HAVE A MEG READING AT A MINIMUM OF 100 MEG-OHMS. ALL EXISTING CIRCUITS TO REMAIN SHALL HAVE A MEG READING, AT A MINIMUM, THAT MATCHES OR IS GREATER THAN THE ORIGINAL MEGGED READINGS. ANY DEFICIENCIES WILL BE CORRECTED BY THE CONTRACTOR AT THEIR EXPENSE TO THE SATISFACTION OF THE RPR.
15. CONTRACTOR SHALL PROVIDE TEMPORARY JUMPER CABLES FOR EXISTING CIRCUITS IN CONSTRUCTION AREAS. ALL TEMPORARY JUMPER CABLES SHALL BE PROTECTED WITH THE BARRICADES ON PAVED AREAS. TEMPORARY JUMPER CABLES SHALL BE INSTALLED ABOVE GROUND IN SCH. 80 PVC CONDUITS. ALL JUMPER CABLES SHALL BE OPERATIONAL DURING CONSTRUCTION. ALL JUMPER CABLES AND CONDUITS SHALL BE REMOVED AT THE COMPLETION OF THE PROJECT. TEMPORARY JUMPER CABLES AND CONDUITS SHALL BE CONSIDERED INCIDENTAL TO THE CABLE AND CONDUITS INSTALLED UNDER THIS PROJECT TEMPORARY WIRING/JUMPERS SHALL NOT BE USED OR INSTALLED IN ANY FINAL INSTALLATION.
16. ALL CONDUIT WILL NOT EXTEND MORE THAN 1-INCH INTO THE INSIDE OF THE LIGHT BASE AND WILL HAVE A RUBBER GROMMET BETWEEN THE CAN AND CONDUIT. A PVC BELL SHALL BE PLACED AT THE ENDS OF THE CONDUIT TO PROTECT CABLES FROM NICKING AND/OR SCRAPING.
17. CABLES INSTALLED IN THIS CONTRACT WILL BE INSTALLED IN SCHEDULE 40 PVC CONDUITS AS INDICATED ON THE PLANS AND DETAILS.
18. THE CONTRACTOR SHALL IDENTIFY AND DOCUMENT ALL CABLES INSTALLED IN EACH DUCT WITHIN THE SCOPE OF THIS PROJECT. THE CONTRACTOR SHALL RECORD THE FINAL CIRCUITRY ON THE "AS-BUILT" DRAWINGS. CONTRACTOR SHALL MAINTAIN RED LINES OF NEWLY INSTALLED MATERIALS AND SHALL UPDATE THEM ON A DAILY BASIS.
19. THE CONTRACTOR SHALL COORDINATE THE ELECTRICAL CIRCUITS WITH THE AIRPORT MAINTENANCE PRIOR TO INSTALLATION AND LABELING.
20. ALL WIRING INSTALLED IN NEW OR EXISTING MANHOLES AND HANDHOLES SHALL BE PROPERLY RACKED AND TAGGED.
21. EXISTING COUNTERPOISE WIRING, GROUND RODS AND CONNECTIONS TO BE REMOVED IN ALL AREAS OF CONDUIT REMOVAL. NEW COUNTERPOISE WIRING, GROUND RODS AND CONNECTIONS TO BE INSTALLED IN ALL AREAS OF CONDUIT INSTALLATION. NEW COUNTERPOISE SHALL BE DIRECT BURIED AND TIED TO EXISTING COUNTERPOISE WITH AN EXOTHERMIC CONNECTION. GROUND RODS SHALL BE PLACED AT MANUFACTURERS AND/OR DUKE ENERGY REQUIRED SPACING. COUNTERPOISE IS NOT SHOWN ON PLAN SHEETS FOR SAKE OF CLARITY BUT SHALL BE REMOVED AND INSTALLED ACCORDINGLY.

REVISIONS:			
REV. NO	DATE	DESCRIPTION:	EOR
	12/12/2022	CONFORMED DOCUMENTS	JCM
	10/12/2023	RECORD DRAWINGS	JCM



FBO/TERMINAL APRON  
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CITY OF AVON PARK  
AVON PARK, FL

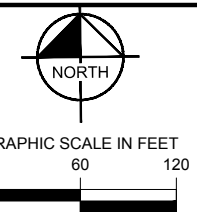
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JOB NO:	046464008
DRAWN:	BJG
DESIGN:	BJG
CHECKED:	JCM
DATE:	3/10/2022

ELECTRICAL NOTES

E001



1. CONTRACTOR TO FIELD VERIFY SIZE, LOCATION, AND DEPTH OF ELECTRICAL SERVICE WITHIN THE PROJECT LIMITS AND COORDINATE ELECTRICAL DEMOLITION WITH RPR PRIOR TO ANY DEMOLITION ACTIVITY PER ITEM L-100-1.
2. ALL UTILITIES SHOWN ARE APPROXIMATE IN NATURE AND ARE ASSUMED LOCATIONS BASED SOLELY ON VISUAL INSPECTIONS. NO SUBSURFACE UTILITY INVESTIGATIONS HAVE BEEN PERFORMED FOR THIS PROJECT.
3. CONTRACTOR TO DEMOLISH EXISTING ELECTRICAL SERVICE AND TEMPORARY BACKFILL, AS SEEN IN DETAIL 1, SHEET E290 DURING PHASE 0 PRIOR TO THE START OF PHASE 1A. NEW ELECTRICAL SERVICE LAYOUT SHALL ALSO OCCUR DURING PHASE 0 PRIOR TO THE START OF PHASE 1A.

— UE — APPROXIMATE UNDERGROUND  
ELECTRIC TO BE REMOVED

— W — APPROXIMATE EXISTING  
WATER LINE

— OHE — APPROXIMATE EXISTING  
OVERHEAD ELECTRIC LINE

REVISIONS:			
REV. NO	DATE	DESCRIPTION:	EOR
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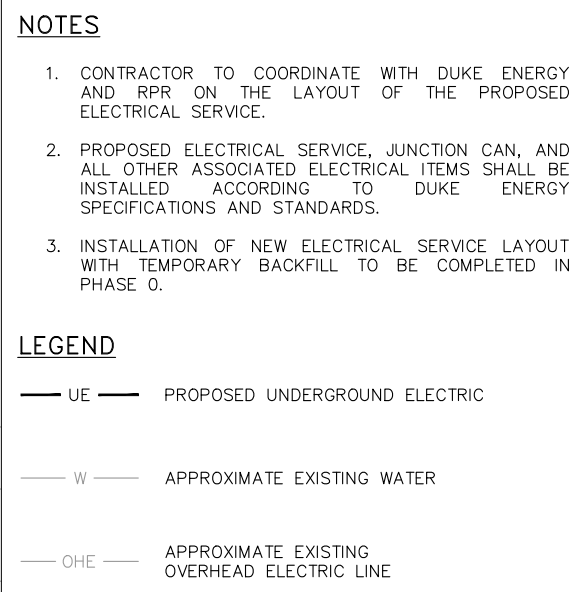
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JOB NO:	046464008	<div> <div>ELECTRICAL DEMOLITION PLAN</div> <div>E110</div> </div>
DRAWN:	BJG	
DESIGN:	BJG	
CHECKED:	JCM	
DATE:	3/10/2022	





REVISIONS:			
REV. NO	DATE	DESCRIPTION:	EOR
	12/12/2022	CONFORMED DOCUMENTS	JCM
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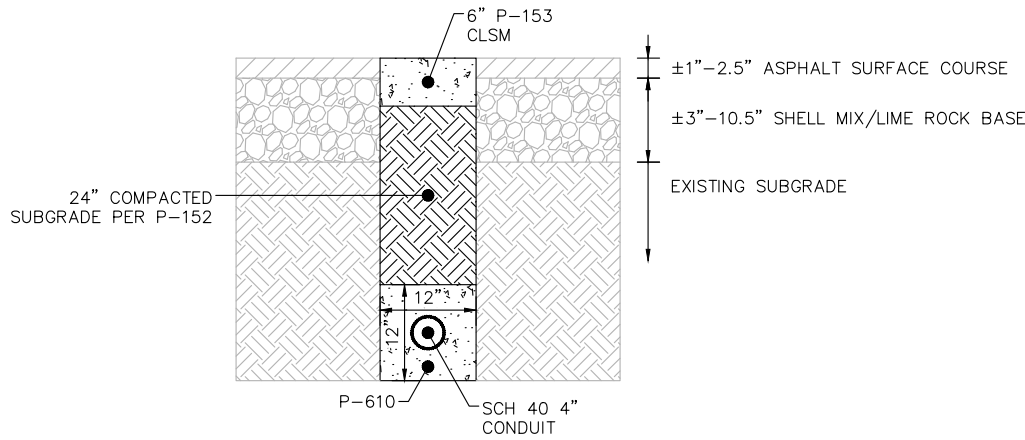


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JOB NO:	046464008	ELECTRICAL LAYOUT PLAN
DRAWN:	BJG	
DESIGN:	BJG	E210
CHECKED:	JCM	
DATE:	3/10/2022	

K:\TAM\_Aviation\1\_Engn\_AVO\2019 - On Call Engineering\Task Orders\FY 2021 - Apron Rehab\CAD\Plan Sheets\DETAILS.dwg Tab: E290 Oct 12, 2023 - 10:09am Plotted by: katie.dodge

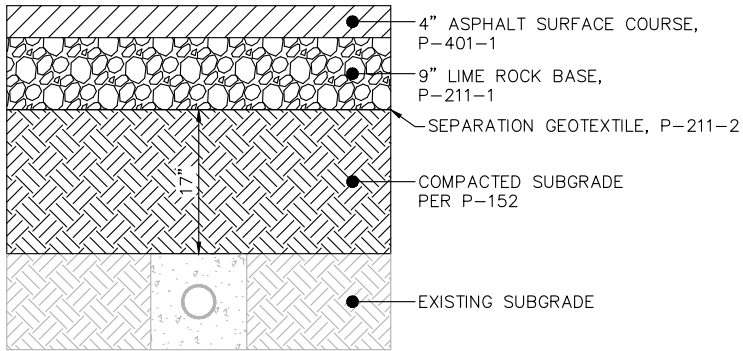


1  
E290

TEMPORARY BACKFILL FOR CONCRETE ENCASED ELECTRICAL SERVICE IN PAVED AREAS

**NOTE:**

1. DURING PHASE 0 (MOBILIZATION) CONTRACTOR SHALL INSTALL NEW ELECTRICAL SERVICE LAYOUT IN ACCORDANCE WITH SHEET E210 FOLLOWING SPECIFICATIONS FROM MANUFACTURER AND/OR DUKE ENERGY. BACKFILL ELECTRICAL SERVICE WITH COMPACTED SUBGRADE AND CLSM AS SEEN IN THIS DETAIL.

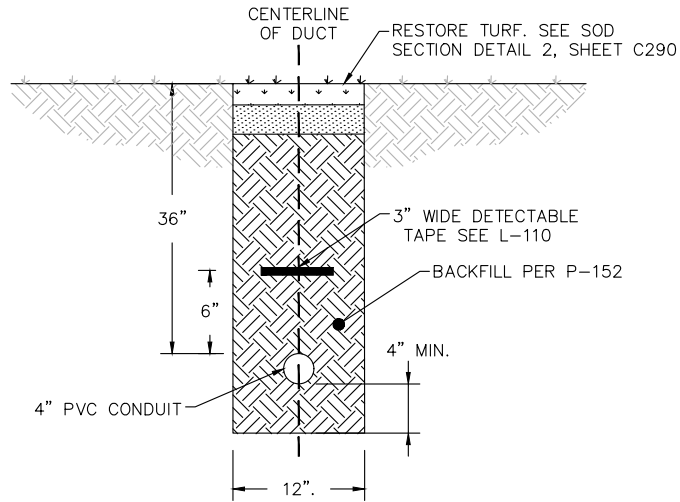


2  
E290

FINAL BACKFILL FOR CONCRETE ENCASED ELECTRICAL SERVICE IN PAVED AREAS

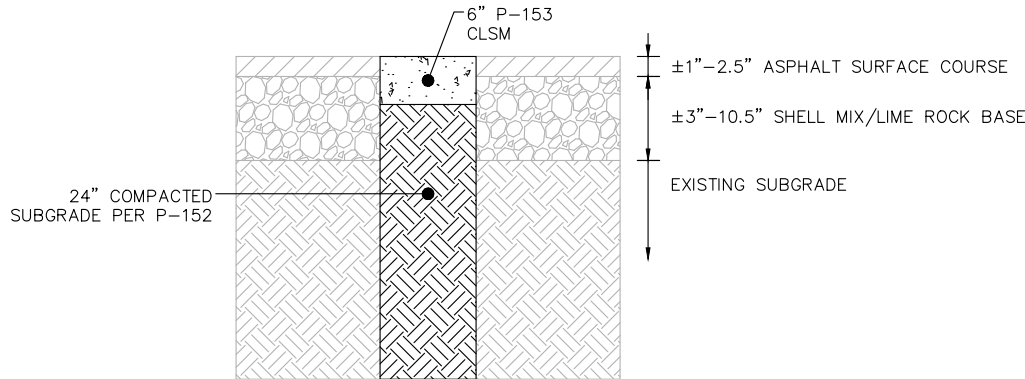
**NOTE:**

1. REMOVAL OF CSLM INSTALLED IN FOR TEMPORARY BACKFILL IN PHASE 0 (MOBILIZATION) SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACTOR'S UNIT PRICE FOR ITEM P-101-1, FULL DEPTH ASPHALT PAVEMENT REMOVAL. INSTALL FINAL BACKFILL SECTION AS SEEN IN THIS DETAIL DURING PAVING OPERATIONS.



3  
E290

BACKFILL FOR NON-ENCASED 4" CONDUIT IN TURF  
NOT TO SCALE



4  
E290

TEMPORARY BACKFILL FOR EXISTING ELECTRICAL SERVICE DEMOLITION IN PAVED AREAS

REVISIONS:			EOR
REV. NO	DATE	DESCRIPTION:	
	12/12/2022	CONFORMED DOCUMENTS	JCM
	10/12/2023	RECORD DRAWINGS	JCM



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JOB NO:	046464008
DRAWN:	BJG
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CHECKED:	JCM
DATE:	3/10/2022

ELECTRICAL DETAILS

E290

**Request for Information (RFI)**

**Project:** Avon Park Executive Airport (AVO) – FBO/Terminal Apron Rehabilitation

**Contractor:** Ajax Paving Ind. of Florida

**RFI #:** 002

**Date:** 3/15/23

**Question:**

See attached.

**Response:**

Based on information from the Contractor and the RPR, it is understood that a portion of the existing t-hanger electrical conduit within the project pavement reconstruction limits is not concrete encased and was found to be at variable depth below finish grade of approximately 6-10". The non-encased conduit begins approximately 10' west of the tie-in location to existing apron pavements.

It is understood that the Contractor proposes to hand-excavate in-situ material below the non-encased conduit to within the pavement reconstruction limits provide a uniform depth of 10" from finish grade of asphalt to the top of the conduit and place 4" P-401 supported by 6" of P-211 above the top of the conduit.

This installation proposed by the Contractor is considered acceptable, provided no additional cost or time to the Contract are incurred, in this area of the project only (within 10' of the tie-in to existing apron pavement as shown on the attached markup of Sheet E210). The Contractor shall ensure that the conduit is protected in place during installation of subsequent pavement section layers.

---

Any direction related to this response is at no additional cost or time to the Project.

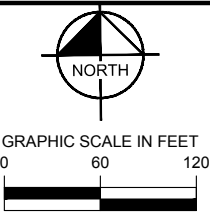
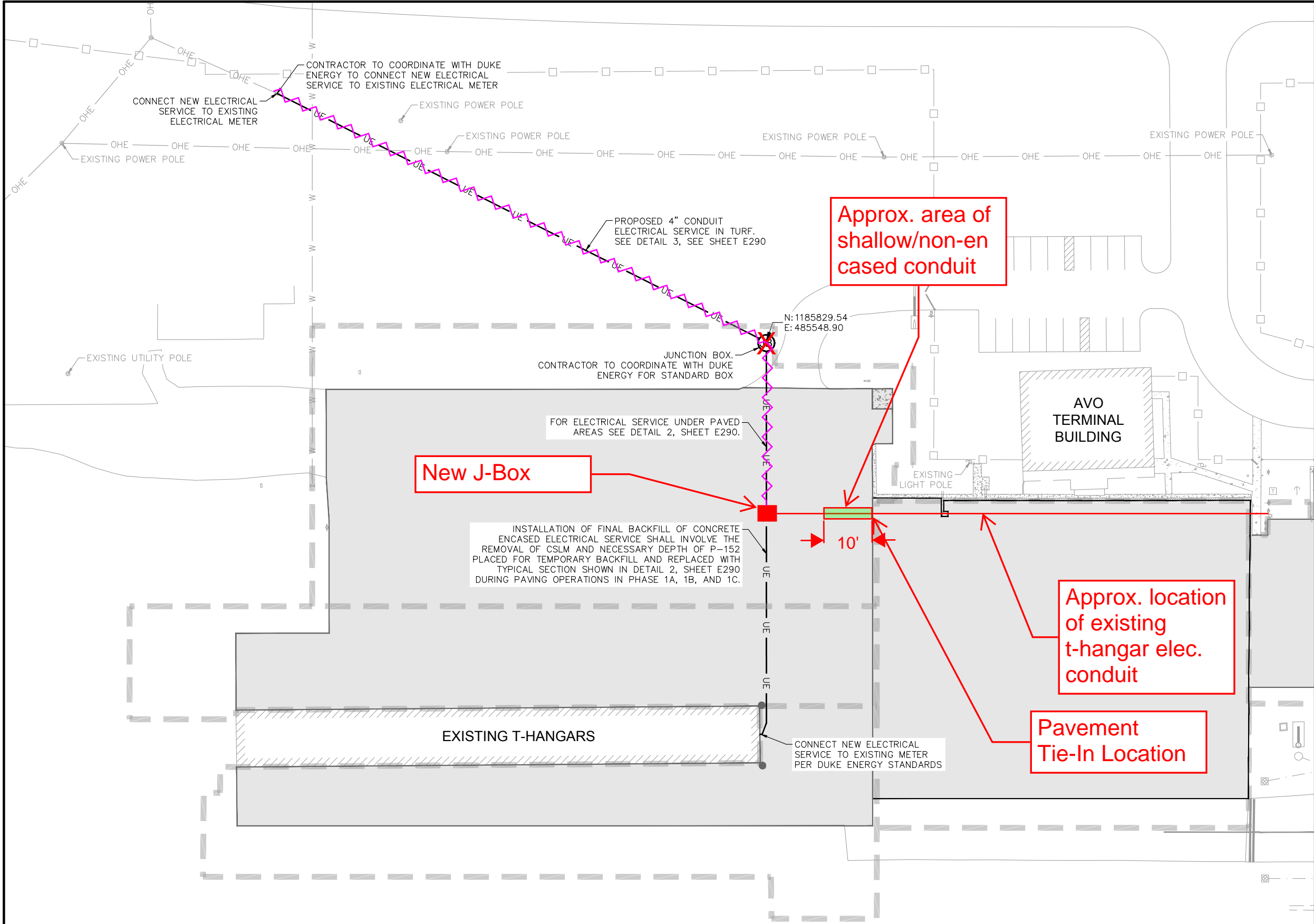
**Issued:**

Engineer: (Name) Jared Moreng, P.E. Date: 3/15/23

(Signature)  \_\_\_\_\_



K:\tam\_aviation\1\_Engr\AVO\2019 - on call engineering\task orders\fy 2021 - apron rehab\CAD\plan sheets\ELECTRICAL.dwg Tab: E200 Dec 20, 2022 - 3:51pm Plotted by: Ashlie.Wells



- NOTES**
1. CONTRACTOR TO COORDINATE WITH DUKE ENERGY AND RPR ON THE LAYOUT OF THE PROPOSED ELECTRICAL SERVICE.
  2. PROPOSED ELECTRICAL SERVICE, JUNCTION CAN, AND ALL OTHER ASSOCIATED ELECTRICAL ITEMS SHALL BE INSTALLED ACCORDING TO DUKE ENERGY SPECIFICATIONS AND STANDARDS.
  3. INSTALLATION OF NEW ELECTRICAL SERVICE LAYOUT WITH TEMPORARY BACKFILL TO BE COMPLETED IN PHASE 0.

- LEGEND**
- UE — PROPOSED UNDERGROUND ELECTRIC
  - W — APPROXIMATE EXISTING WATER
  - OHE — APPROXIMATE EXISTING OVERHEAD ELECTRIC LINE

REVISIONS:			EOR
REV. NO	DATE	DESCRIPTION:	
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JOB NO:	046464008
DRAWN:	BJG
DESIGN:	BJG
CHECKED:	JCM
DATE:	3/10/2022

ELECTRICAL LAYOUT PLAN

**E210**



*The Future is Riding on Ajax.™*

## Request for Information

RFI No.: ~~-001-~~ 002

Date: 3/14/2023

---

**Project:** AVO – FBO/Terminal Apron Rehabilitation  
**Ajax Project No.:** 406122  
**Issued To:** Jared Moreng, P.E.

---

**Subject:** RFI No. ~~-001-~~ 002  
**Reply Requested By:** 3/16/2023

---

**Background:** The original electrical design was modified to tie into the existing east-west conduit run at junction box. During the installation of the junction box it was determined that the conduit is very shallow at the beginning of the run and the goes deeper. It was just discovered that the concrete has broken off in shallow area and the conduit is not encased.

**Request:** Ajax is requesting direction on how we should handle this issue. We feel the best course of action would be to leave the conduit in the existing location, ensuring that we maintain a minimum of 10" above the conduit. This will allow for a 6" lift of limerock base and 4" lift of asphalt.

---

**Cost Impact:** No change to cost

---

**Submitted By:**

Sharon Radford, Project Manager – AJAX Paving

---

**Owner/Engineer Response:**

---

Response By: \_\_\_\_\_

Date: \_\_\_\_\_

**Ajax Paving Industries of Florida, LLC**  
One Ajax Drive, North Venice, FL 34275

Main: 941-486-3600 Fax: 941-486-3500  
[www.ajaxpaving.com](http://www.ajaxpaving.com)

*An Equal Opportunity Employer*

### Engineer's Review of Shop Drawings

**Project:** Avon Park Executive Airport (AVO) – FBO/Terminal Apron Rehabilitation

**Contractor:** Ajax Paving Industries of FL

**Submittal Title:** Final As-Built Survey

**Submittal No.:** 022

**Action Taken:**

- ☒ No Exceptions Taken
- ☐ Make Corrections Noted,  
No Resubmittal required
- ☐ Revise and Resubmit
- ☐ Rejected
- ☐ Not Required for Review

Our limited review is only for compliance with the design concept and information given in the Contract Documents. Contractor is solely responsible for dimensions, quantities, performance, safety coordination with other work, and all other requirements of the Contract Documents. Review does not authorize changes to contract unless stated specifically in separate letter or change order.

Reviewed By: Jared Moreng, P.E.

Date: 05/23/23

**KIMLEY-HORN AND ASSOCIATES, INC.**



By: \_\_\_\_\_

**Notes:**



**SUBMITTAL 022 – Final Asbuilts****AJAX PAVING INDUSTRIES of FLORIDA, LLC****Area 4**

24174 Hwy 27 Unit 300  
Lake Wales, FL 33859

Phone 239-936-9444

Fax 239-936-9445

Cell 863-270-5284

email [sradford@ajaxpaving.com](mailto:sradford@ajaxpaving.com)

Date	4/28/2023
Attn	Jared Moreng
Company	Kimley - Horn
From	Ajax Paving
Project Name	AVO – FBO/Terminal Apron Rehabilitation

**WE ARE SENDING YOU THE FOLLOWING:**

Copies	Date	Description
1	4/28/2023	<b>Final Asbuilts</b>

**Comments:**

Please see the attached for review and approval.

<p><b><u>APPROVED FOR SUBMITTAL</u></b> Ajax Paving Industries of Florida, LLC</p> <p>Project Manager</p>
---------------------------------------------------------------------------------------------------------------

Emailed to:  
Jared Moreng  
**Kimley-Horn**

# CITY OF AVON PARK

## FBO/TERMINAL APRON REHABILITATION

### *AS-BUILT SURVEY*

DECEMBER 12, 2022

FAA A.I.P. NO. 3-12-004-022-2022

#### AS-BUILT CONTROL POINTS

5/8" IRON ROD & CAP  
STAMPED BFA TRAV  
N: 1185480.94  
E: 485578.67  
EL = 152.74  
(NAVD 1988)

5/8" IRON ROD & CAP  
STAMPED BFA TRAV  
N: 1185674.21  
E: 485184.11  
EL = 152.78  
(NAVD 1988)



#### PROJECT LOCATION

N.T.S.

#### ENGINEER OF RECORD

JARED MORENG, FL P.E. NO. 81611 - FBO/TERMINAL APRON REHABILITATION

PREPARED FOR:



PREPARED BY:

**Kimley»Horn**  
Expect More. Experience Better.

KIMLEY-HORN AND ASSOCIATES, INC.  
REGISTRY NO. 35106  
655 NORTH FRANKLIN STREET, SUITE 150  
TAMPA, FL 36602-4447  
813-620-1460

#### AS-BUILT SURVEY LEGEND

LB	LICENSED BUSINESS
LS	LICENSED SURVEYOR
BM	BENCHMARK
N:	NORTHING
E:	EASTING
EL	ELEVATION

#### NOTES:

THIS SURVEY WAS PREPARED AS AN AS-BUILT SURVEY.

AS-BUILT INFORMATION SHOWN IN RED.

SURVEY BASELINE, BENCHMARKS AND CONTROL POINTS WERE PROVIDED BY THE CLIENT IN CONSTRUCTION PLANS, CAD FILES PREPARED BY KIMLEY HORN AND ASSOCIATES, INC, DATED: DECEMBER 12, 2022.

STATIONS, OFFSETS, COORDINATES AND ELEVATIONS ARE BASED ON THE CONSTRUCTION CONTROL DATUMS PROVIDED IN SAID CONSTRUCTION PLANS.

ABOVE GROUND AND UNDERGROUND IMPROVEMENTS OTHER THAN AS SHOWN WERE NOT LOCATED.

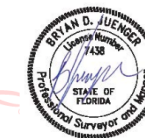
LAST DATE OF FIELD WORK: 04-14-2023

#### SURVEYOR'S CERTIFICATION:

I HEREBY CERTIFY TO THE BEST OF MY PROFESSIONAL KNOWLEDGE, THAT THIS PLAT OF THE HEREON DESCRIBED PROJECT IS A TRUE REPRESENTATION OF A FIELD SURVEY MADE UNDER MY DIRECTION AND CONFORMS TO THE STANDARDS OF PRACTICE FOR PROFESSIONAL SURVEYORS AND MAPPERS AS OUTLINED IN CHAPTER 5J-17, FLORIDA ADMINISTRATIVE CODE.

DIGITALLY SIGNED AND SEALED:

Digitally signed by  
Bryan Juenger  
Date: 2023.05.17  
'10:00:08 -04'00



BRYAN JUENGER  
PROFESSIONAL LAND SURVEYOR  
FLORIDA LICENSE NO. LS7438

DATE

#### NOTICE:

UNLESS ELECTRONICALLY SIGNED AND SEALED, THIS SURVEY IS NOT VALID WITHOUT THE SIGNATURE AND THE ORIGINAL RAISED SEAL OF A FLORIDA LICENSED SURVEYOR AND MAPPER.

ADDITIONS OR DELETIONS TO SURVEY MAPS OR REPORTS BY OTHER THAN THE SIGNING PARTY OR PARTIES IS PROHIBITED WITHOUT WRITTEN CONSENT OF THE SIGNING PARTY OR PARTIES.



**A.A. SURFACE PRO**

4409 SE 16TH PLACE, UNIT 6  
Cape Coral, FL 33904

Tel: 239.362.4882  
CERTIFICATE OF AUTHORIZATION #L.B.8309

DATE	JOB#	DWG	SCALE	SHEET
04-21-2023	23-003	23-003 AVON PARK ASB	AS SHOWN	AS SHOWN

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REVISIONS:			EOR
REV. NO	DATE	DESCRIPTION:	
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CITY OF AVON PARK  
AVON PARK, FL

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# A.A. SURFACE PRO

4409 SE 16TH PLACE, UNIT 6  
Cape Coral, FL 33904  
Tel: 239.362.4882  
CERTIFICATE OF AUTHORIZATION #L.B.8309

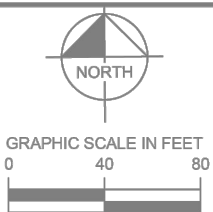
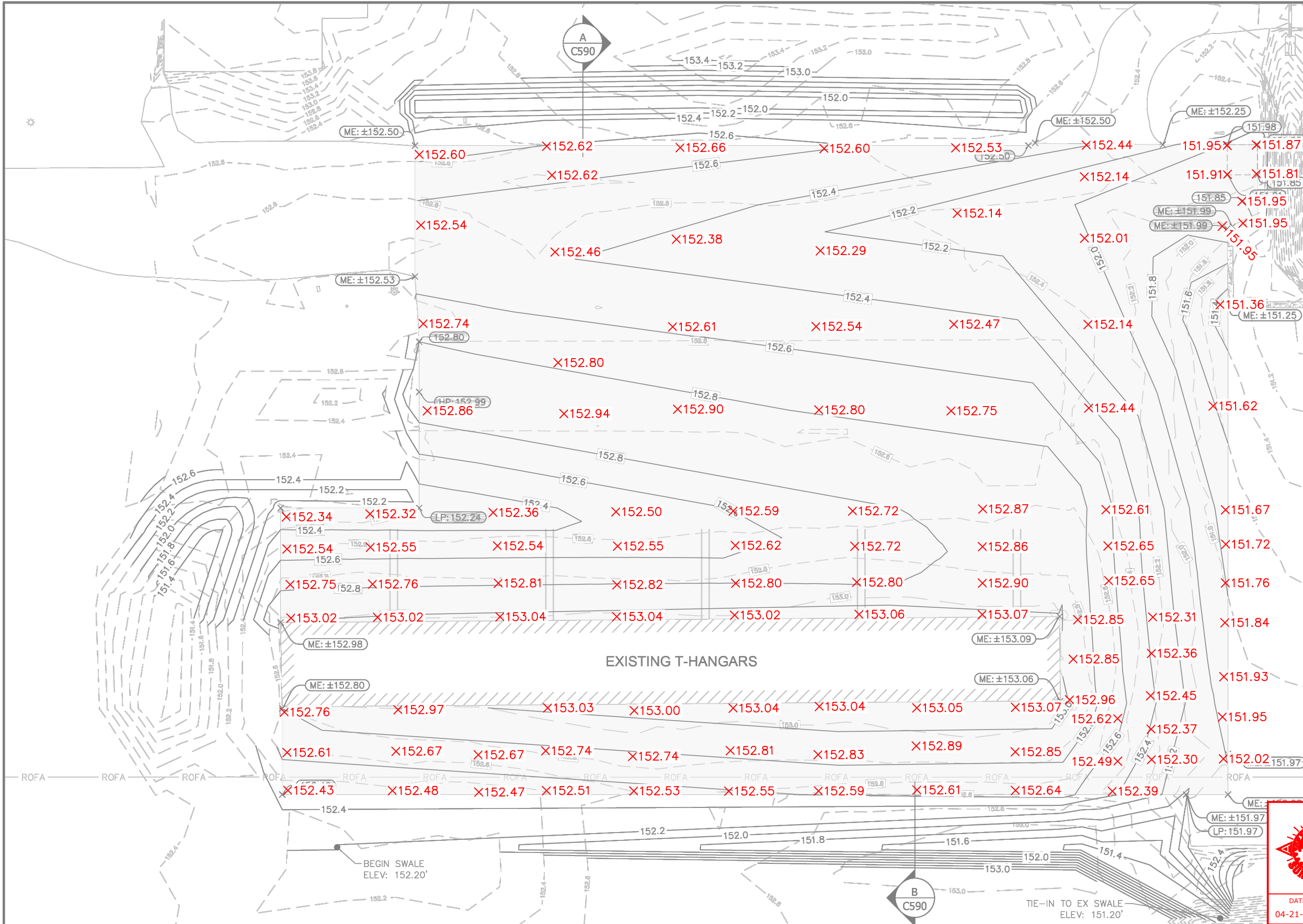
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JOB NO:	046464008
DRAWN:	BJG
DESIGN:	BJG
CHECKED:	JCM
DATE:	3/10/2022

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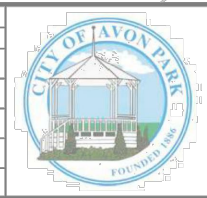
NOTES

1. SEE SHEET C410 & C411 FOR PAVEMENT GEOMETRY.

LEGEND

- PROPOSED PAVEMENT
- X (ME: ±152.20) MATCH EXISTING ELEVATION
- X (LP: 152.45) LOW POINT ELEVATION
- X (HP: 152.45) HIGH POINT ELEVATION
- X (152.60) PROPOSED SPOT ELEVATION
- 152.0 PROPOSED CONTOUR
- 152.0 EXISTING CONTOUR

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04-21-2023	23-003	23-003 AVON PARK ASB	AS SHOWN	AS SHOWN

JOB NO: 046464008

DRAWN: BJC

DESIGN: BJC

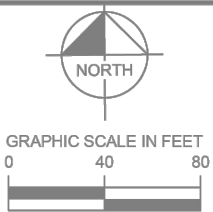
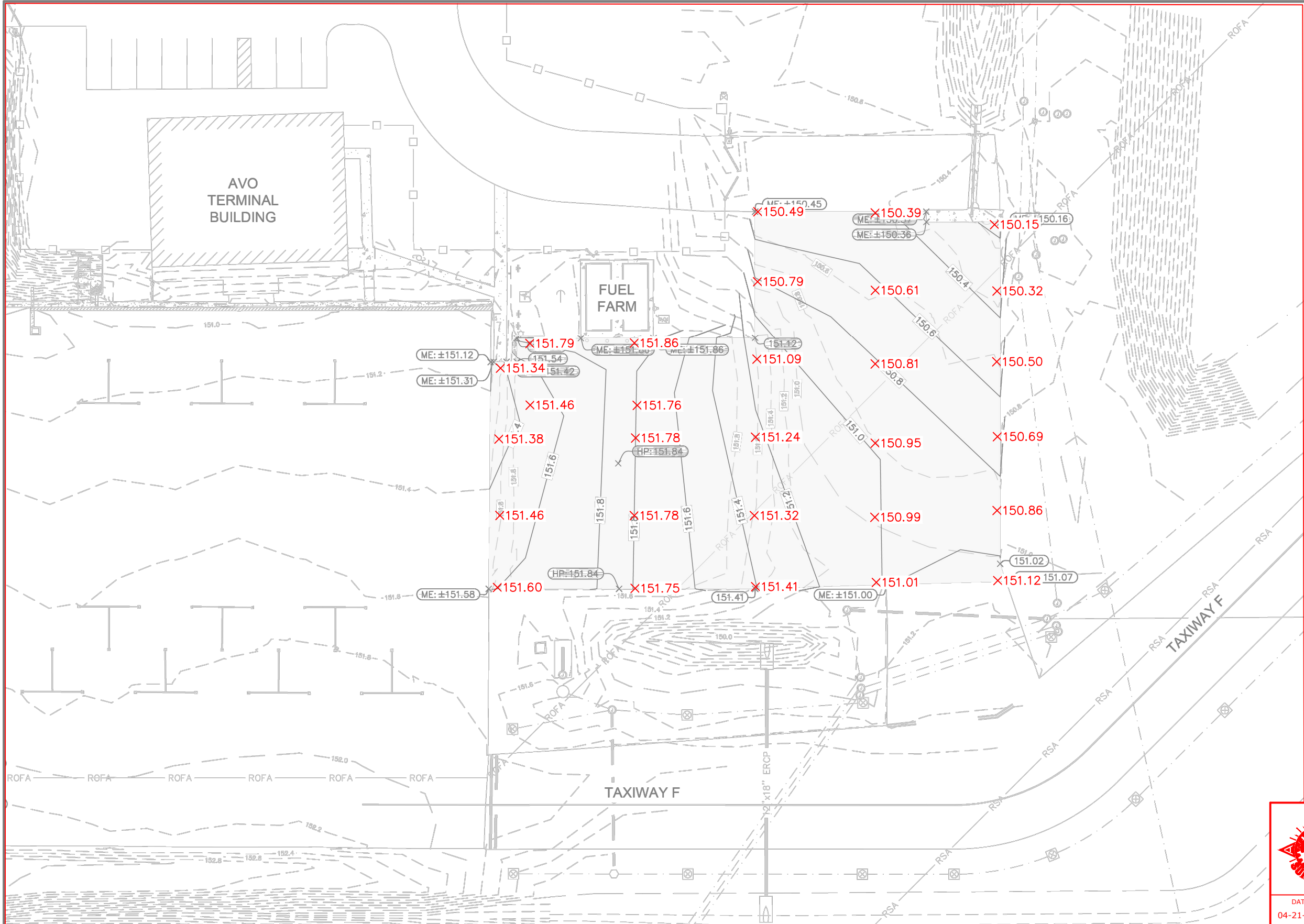
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GRADING PLANS

**C510**

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**NOTES**  
1. SEE SHEET C410 & C411 FOR PAVEMENT GEOMETRY.

- LEGEND**
- PROPOSED ASPHALT PAVEMENT
  - MATCH EXISTING ELEVATION
  - PROPOSED SPOT ELEVATION
  - PROPOSED CONTOUR
  - EXISTING CONTOUR

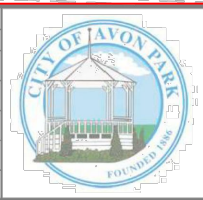


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JOB NO:	046464008	GRADING PLANS
DRAWN:	BJG	
DESIGN:	BJG	C511
CHECKED:	JCM	
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